

CARLYLE



Alexandria, Virginia

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Streetscape Design Guidelines



Streetscape Design Guidelines

Carlyle Development Corporation

Prepared by:

**LandDesign, Inc.
April, 1994**

...The people on the sidewalk, being culture-bound, know that the space which they are part of is a sidewalk, and, as part of their culture, they have the pattern of a sidewalk in their minds. It is this pattern in their minds which causes them to behave the way that people do behave on sidewalks, not the purely spatial aspect of the concrete and the walls and curbs.

— Christopher Alexander

CARLYLE STREETScape DESIGN GUIDELINES

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1 INTRODUCTION

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1 INTRODUCTION

1.1 CARLYLE STREETSCAPE DESIGN GUIDELINES

Carlyle is a eighty-two acre mixed use development in Alexandria, Virginia comprised of office, residential, hotel, retail, and recreational uses. It is located between Old Town and the Eisenhower Valley, and adjacent to the King Street Metro Station area. The development process for the site began with the submission of a Development Plan prepared by Cooper, Robertson and Partners to the City of Alexandria. The Development Plan applied for approval for rezoning the property to CO zoning, for a special use permit for a planned residential and commercial development, and a transportation management plan special use permit. The Development Plan was approved by the Alexandria City Council in April 1990 with conditions. Compliance with these conditions and the Development Plan requirements will be required by the City of Alexandria for all site plan submissions.

The Development Plan and the conditions are also the basis for this document - the Carlyle Streetscape Design Guidelines.

The purpose of the Carlyle Streetscape Design Guidelines is to ensure high quality, integrated, compatible development while allowing flexibility over time to address changing development needs. Six precincts have been designated within Carlyle to reflect one of the principal goals of the Development Plan — that of creating a series of distinct places. These guidelines will describe the desired streetscape characters for Carlyle's six precincts. To achieve the desired characters, the guidelines will provide standards and specifications to guide parcel owners with the design and construction of the streetscape. The streetscape design principles will also be described to give a basis for future interpretation by users of these guidelines.

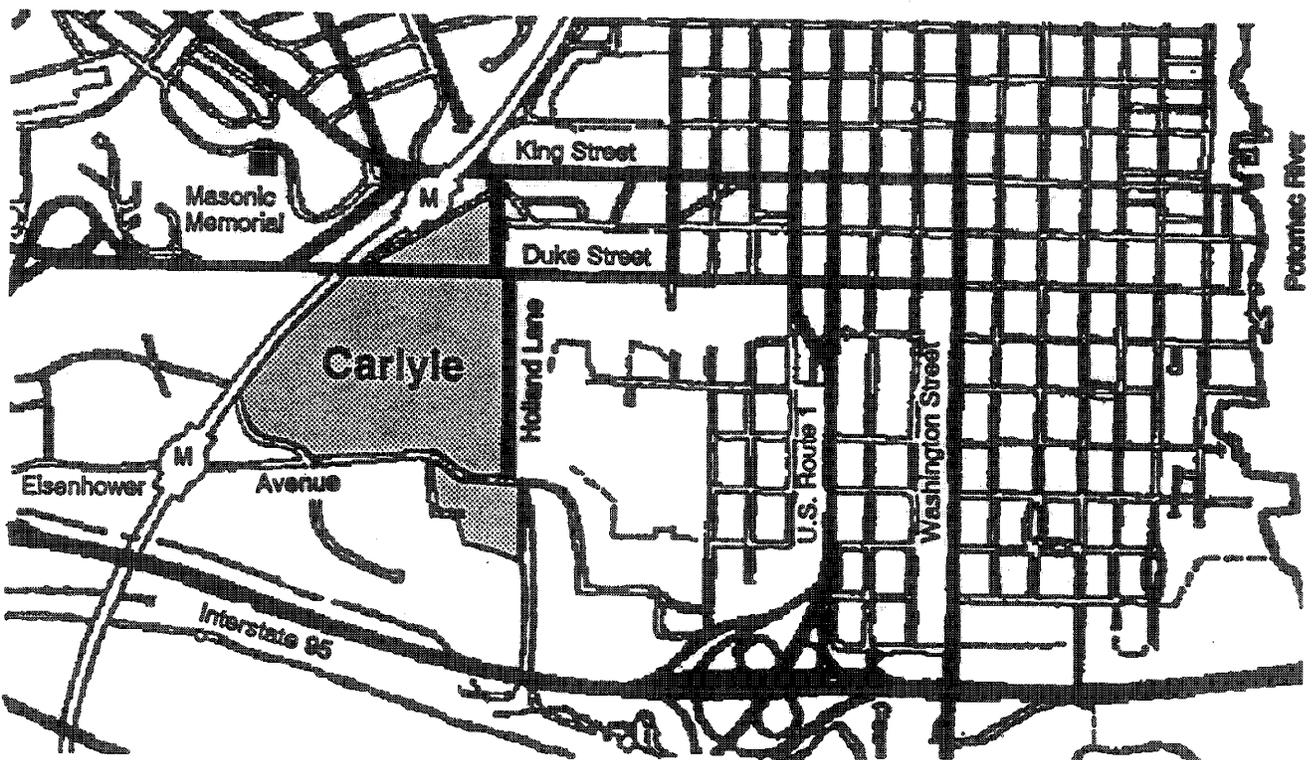
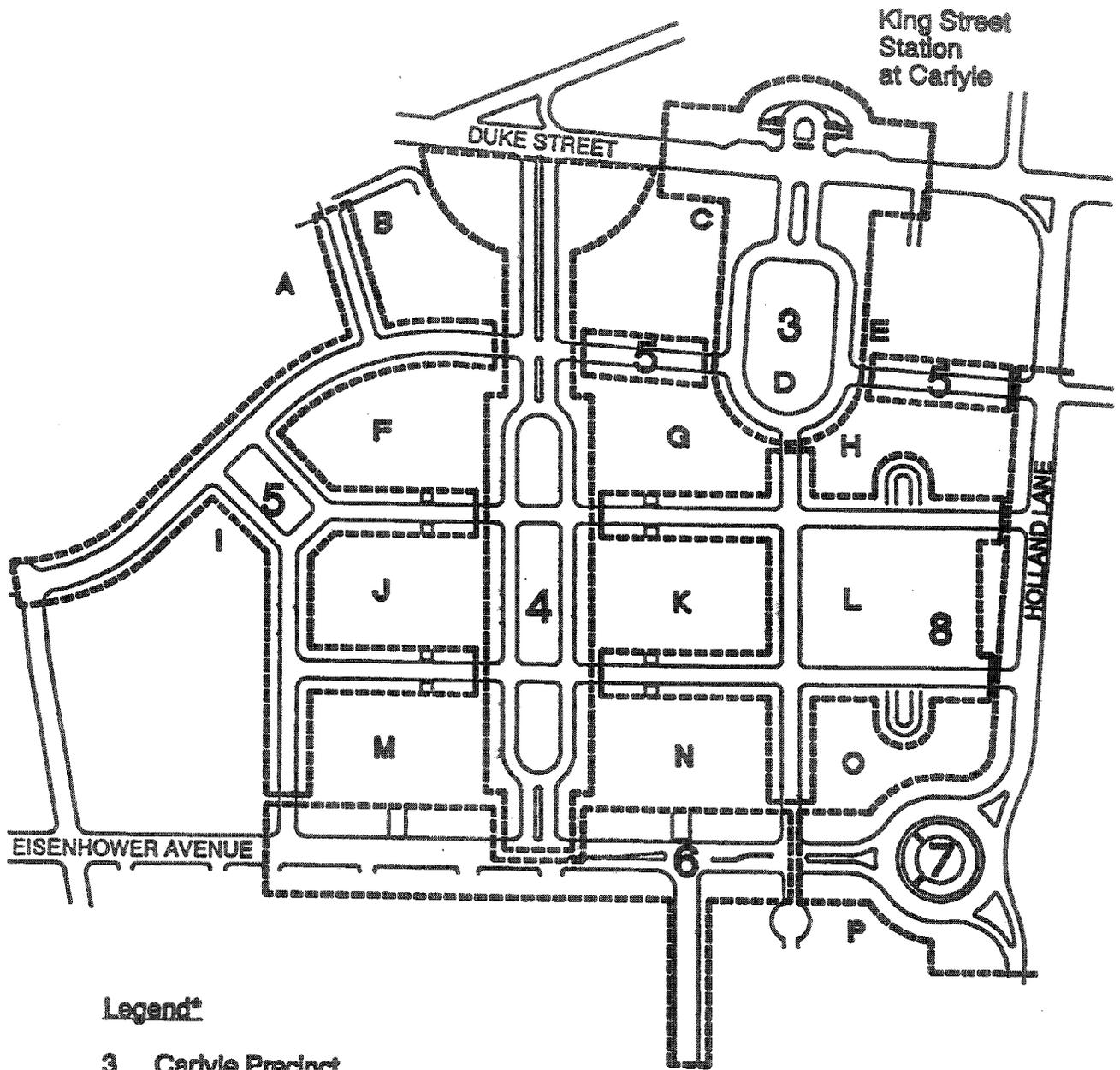


Figure 1-1: Vicinity Map



Legend*

- 3 Carlyle Precinct
- 4 Dulany Precinct
- 5 Jamieson Precinct
- 6 Eisenhower Precinct
- 7 Holland Precinct
- 8 Ballenger Precinct

* Numbers correspond to appropriate chapters

Figure 1-2: Precinct Plan



1.2 ORGANIZATION AND USE OF THE GUIDELINES

1.2.1 ORGANIZATION

There are eight chapters of the Carlyle Streetscape Design Guidelines. This, the first, is a basic introduction to the guidelines. Chapter Two contains guidelines that apply uniformly to all of Carlyle. Following Chapter Two are six chapters, one for each precinct. These six chapters address specific requirements for each precinct. The precinct chapters will not repeat the general guidelines in Chapter Two. If a topic is not addressed in the precinct chapter, the general guidelines will apply.

Each precinct chapter of the guidelines is intended to be used independently from the others. For example, the parcel owner for a site in the Jamieson Precinct would refer to the General Design Guidelines (Chapter Two) and the Jamieson Precinct Guidelines (Chapter Five).

1.2.2 DESIGN REVIEW

Carlyle Development Corporation or its agents will review all site and building development plans for conformance to these design guidelines, prior to the plans' submission to the Carlyle Design Review Board.

Carlyle Development Corporation may amend these guidelines as necessary to adhere to the design intent. Carlyle Development Corporation may also approve variances to these guidelines for a specific plan submission provided that the requested variance does not conflict with the design intent stated herein. Decisions for variances are made on an individual basis and do not constitute a change in the requirements of the guidelines.

The Carlyle Design Review Board has been established as a condition of the Development Plan (see S.U.P. conditions #R-67 to R-73).

1.2.3 APPLICATION AND RESPONSIBILITIES

These guidelines apply to all development and construction in the open spaces, street rights-of-way and sidewalk and landscape easements within Carlyle. All repairs to the streetscape due to utility maintenance shall be made in accordance to these guidelines as well.

These guidelines are based on the Development Plan and Special Use Permit #2253 as approved by the Alexandria City Council. The Development Plan and Special Use Permit includes the following documents:

- Executive Summary
- Demolition Plan/Existing Conditions
- Preliminary Site Plans
- Landscape Plans
- Block by Block Design Guidelines
- Conditions as approved by the City of Alexandria

Conformance to these streetscape guidelines does not relieve the developer of compliance to the Development Plan and the conditions of the Special Use Permit or any other applicable City, State or Federal codes, ordinances and regulations.

Generally, the parcel owner shall be responsible for installing all streetscape improvements in the public right-of-way from the back of curb and in the sidewalk and landscape easements. The City of Alexandria will maintain the sidewalks within the public right-of-way except for landscaping. The streetscape outside of the public right-of-way and the landscaping within the public right-of-way will be maintained by the Carlyle Property Owners Association, unless noted otherwise (see S.U.P. Conditions #R-1 and R-15).

When specific products are specified in these guidelines, equivalent substitutions may be made with the approval of Carlyle Development Corporation.



1.3 DEFINITIONS

BUILD-TO LINE: The line as set by the sidewalk easements and described in the Block by Block Design Guidelines as the mandatory streetwall.

BUILDING ZONE: The zone of transition from the building wall to the sidewalk. The building zone may include plantings, architectural features, entrances, steps, awnings, seating, et cetera.

CARR DEVELOPMENT & CONSTRUCTION: Carr Development & Construction is the developer of Carlyle.

CARLYLE DEVELOPMENT CORPORATION: Carlyle Development Corporation is the owner of Carlyle.

CARLYLE PROPERTY OWNERS ASSOCIATION: The association of all property owners within the area of the Carlyle Special Use Permit except those specifically excepted.

CURB ZONE: The zone of transition from the sidewalk to the street - therefore containing the limits of the sidewalk by a curb. This zone contains most of the utilitarian fixtures of an urban street such as fire hydrants, street lights and street trees.

OPEN SPACES: In these guidelines, the term open spaces shall refer to the seven major public squares and parks: Courthouse Square, the Crescent, Dulany Gardens, Carlyle Square, Ballenger Mews, the Rotary and Alexandria African-American Heritage Park.

PARCEL OWNER: In these guidelines, the term parcel owner shall include the fee simple owner, developer, lessor, or any other person or entity constructing improvements to property within Carlyle.

PEDESTRIAN ZONE: The zone of concentrated pedestrian movement along a sidewalk, bound by the building zone on one side and the curb zone on the other. The zone is generally free of obstacles but may have encroachments of planting, seating or directories.

PRECINCT: An area of specified character and design within Carlyle.

RIGHT-OF-WAY (R.O.W.): The public dedicated portion of the street. Right-of-way as used in these guidelines means the perpetual surface easements of the streets in Carlyle as well.

SIDEWALK: The pedestrian area between the curb and the building streetwall.

SIDEWALK EASEMENT: As defined in the Development Plan submission Executive Summary:

"A landscape area, or sidewalk widening within the [parcel] property line to ensure adequate and uniform sidewalk widths....It is a continuous open area at the same level as the adjoining sidewalk and is directly accessible to the public. It may include, on wider sidewalks, obstructions such as driveways, canopies, trees or other landscape elements."

SPECIAL USE PERMIT (S.U.P.): The approval granted by the City of Alexandria for rezoning and development of planned residential and commercial development. In these guidelines, it shall refer to Special Use Permit #2253 as approved by the City of Alexandria in April 1990, and subsequently amended for Carlyle.

STREETSCAPE: The area visually and physically part of the street encompassing the public right-of-way, the sidewalk easements and adjoining parcels.



1.4 BIBLIOGRAPHY

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2 GENERAL STREETScape GUIDELINES

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2 GENERAL STREETScape GUIDELINES

This chapter addresses overall design issues and standards that apply to the entire Carlyle development. Many of the following sections will be referenced throughout the guidelines.

2.1 DESIGN CONCEPT AND CHARACTER

The design concept for Carlyle is based upon traditional city and town planning principles as stated in the Development Plan Design Report:

The plan calls for the site to be an extension of the existing city, [Alexandria] with a traditional system of streets and blocks derived from the Old Town grid. The plan avoids uniformity, incorporating different open spaces and streets, many blocks and different building types to create variety and complexity. It is a return to a more traditional model of town planning and city design [rather] than the current large scale commercial development found in the region.

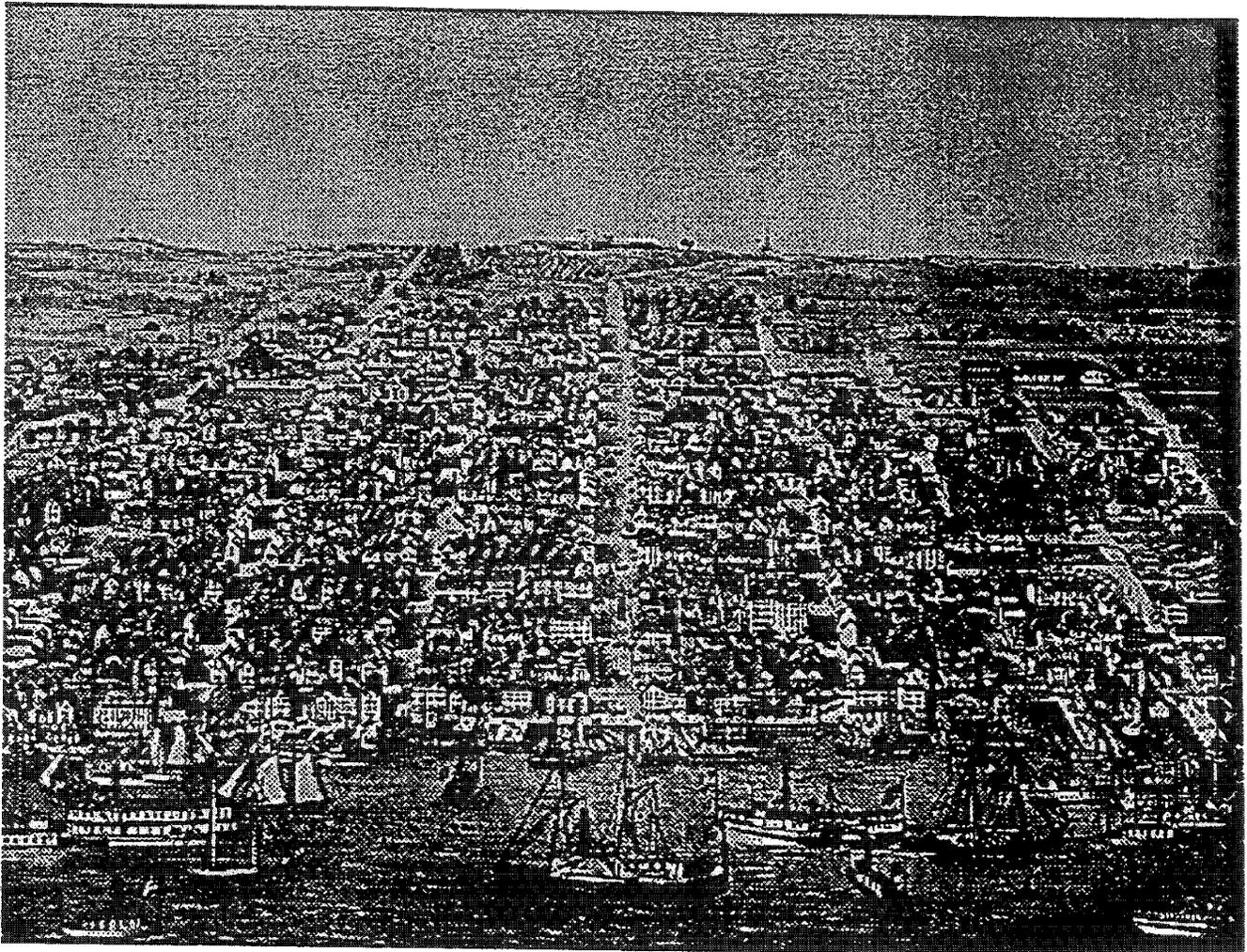


Figure 2-1: Alexandria, 1863



Six design principles are established in the plan:

1. A mix of uses within each precinct, and an effort to maximize active ground floor frontages.
2. A constantly varied sequence of open spaces which become the focus of each precinct in the plan. The plan will have six places of special character.
3. An organization of the plan into a series of pedestrian-oriented places. The control of the physical impacts of the car and parking facilities to clearly emphasize the public space. A public design that reinforces the pedestrian-oriented character of the plan.
4. The introduction of a series of public amenities and facilities.
5. The role of the street as linear park, multi-purpose corridor and ordering device for the street and block grid.
6. Conscious effort to diversify the kinds and numbers of buildings, and the variety of scales and architectural expressions that will result. The scale and density within each block is consciously controlled. A clear building height rationale reinforces the streets and special places of the plan.

These design principles focus the development of outdoor spaces for the use and enjoyment of pedestrians. The design character is developed through the use of streetscape elements, such as street trees, sidewalk paving, street lighting, open space features and street furniture. In each precinct, the site context, its specific uses and intended imagery should be considered for streetscape elements that will reinforce the Precinct character.

2.1.1 CONTEXT

The development of specific streetscapes and precinct characters should consider context in the initial analysis of design. Context - the configuration and scale relationships of streets and intersections, buildings, and major open spaces - establish the physical setting for streetscape design. These components and their scale relationships distinguish the context from one precinct to another. For example, Ballenger Precinct's residential buildings are generally lower in height, and scaled for the pedestrian. The residential buildings reinforce the home-like character and define the outer limits of the mews. A major open space may influence the context depending upon the proximity of the space and the specific elements used within the open space and the streetscape. The closer the space and the more similar its elements, the greater its influence. For example, the streetscapes directly adjacent to a major open space usually share common street furniture, street trees and street lighting. The streets not adjacent to the open space may have related streetscape elements but they do not need to be identical.

2.1.2 USE

The character of a place is influenced by specific uses. The type of use adjacent to the sidewalks (public, semi-public and private) has varying degrees of influence on the streetscape environment. Public uses in open spaces such as parks and squares may interrupt and alter the adjacent streetscape. Semi-public places such as building entrances, lobbies, courtyards and residential gardens will be more integrated into the streetscape. Private uses such as a single residential entrance or garden would only subtly alter the character or configuration of the streetscape. The scale of the streetscape also corresponds to the type of use: public uses usually have wider sidewalks and more street furniture than private uses.

Many combinations of specific types of uses are possible: public oriented retail, private residential and semi-public open spaces. There are two categories of general uses that determine the precincts, predominant use and major first floor use. In most cases, a predominant use such as

PRECINCT	MAJOR FIRST FLOOR USE	PREDOMINANT USE
Carlyle	Retail	Mixed-residential/office/retail
Dulany	Office/residential	Office
Jamieson	Retail	Office
Eisenhower	Office	Office
Holland	Residential	Residential/park
Ballenger	Residential	Residential

Figure 2-2: Uses in Carlyle

residential or office establishes the precinct and influences the streetscape character. However, a special first floor use such as retail may change the character of the streetscape and redefine the precinct. Precincts and their characters may change if the predominant or first floor use is significantly modified. The chart below lists both categories of uses for each precinct. Uses that create strong indoor to outdoor relationships such as sidewalk cafes, book stores, haberdasheries, boutiques, ice cream parlors, florists and news stands are pedestrian oriented and typically generate concentrated pedestrian movements along the streets and to the buildings. The architectural response to this pattern is reflected by the large window displays, multiple entrances, and pedestrian scaled detailing. The streetscape accommodates the increased pedestrian use with wide sidewalk areas, benches, directional signs, awnings and canopies, and other street furniture.

Evening uses that extend beyond normal business hours include restaurants, shops, night clubs, movie theaters and community buildings. The streetscape environment should accommodate vehicular and pedestrian traffic with additional safety considerations of adequate street lighting, designated parking areas, and additional lighting of public open spaces.

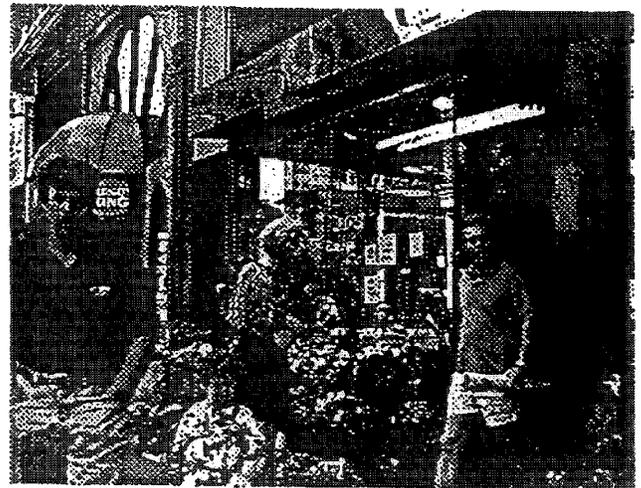


Figure 2-3: Indoor/Outdoor Relationships



Figure 2-5: Carlyle Precinct Image

2.1.3 IMAGERY

Although context and use characterize a precinct, the specific designs and landscape elements for the major open spaces and streetscapes provide the visual images that will most differentiate one neighborhood or precinct to another. The following chart outlines the images and character for each precinct. Each precinct chapter will further discuss its imagery.

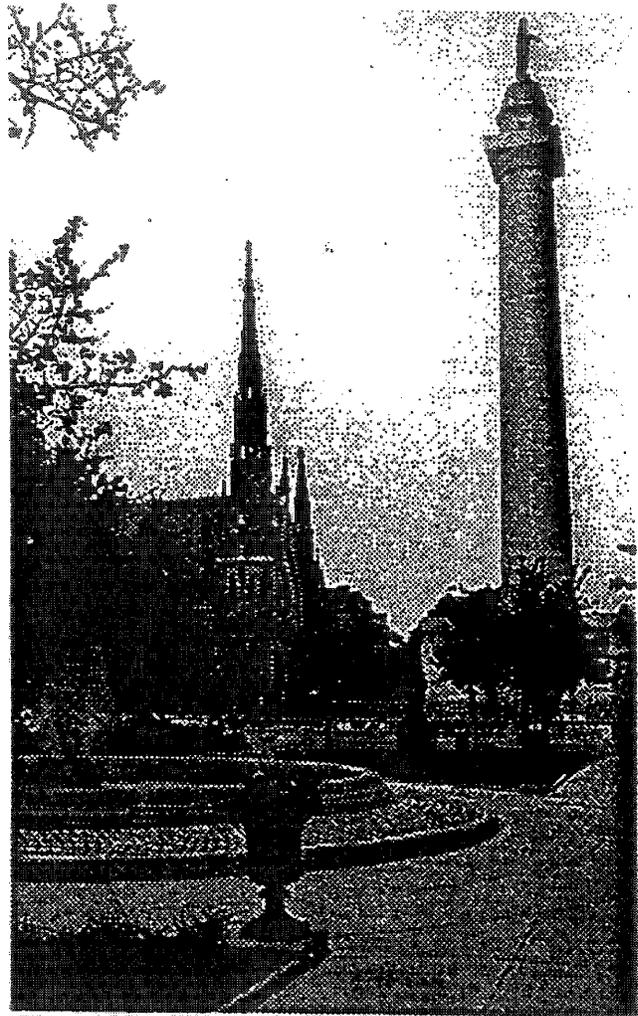


Figure 2-6: Dulany Precinct Image

PRECINCT	IMAGES	CHARACTER
Carlyle	Street vendors, outdoor cafe, shopping, festive	Marketplace
Dulany	Rich materials, formal gardens, grand architecture with distinction	Ceremonial
Jamieson	Federal Courthouse, urban plaza, central open space, authoritative	Civic
Eisenhower	Double row of trees, large grass panels, broad street, monumental	Boulevard
Holland	Large existing trees, pastoral landscape, passive park, picturesque	Parkway
Ballenger	Intricate detailing, variety, lush open space, human scale	Home

Figure 2-4: Precinct Images and Character

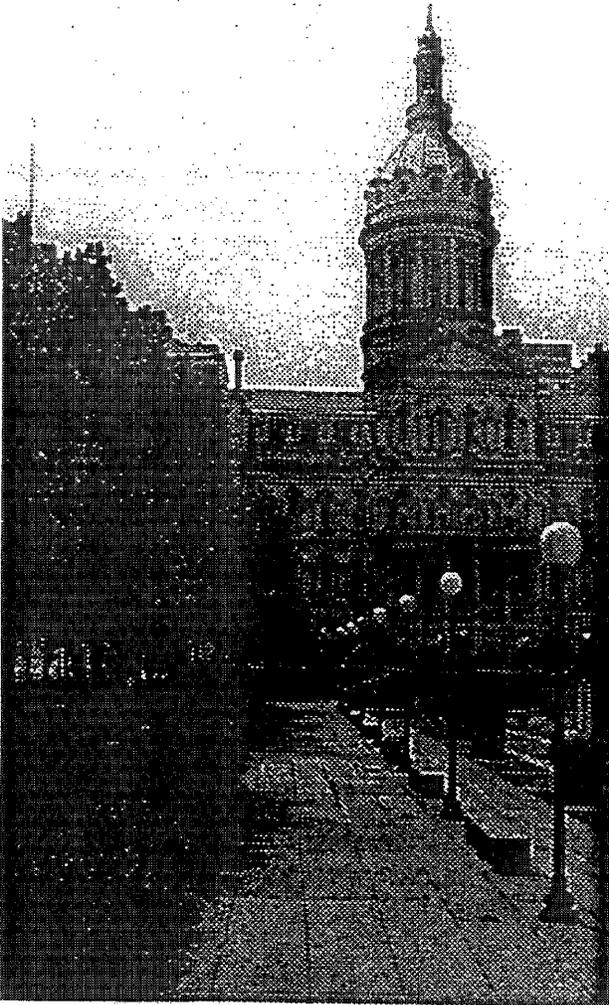


Figure 2-7: Jamieson Precinct Image

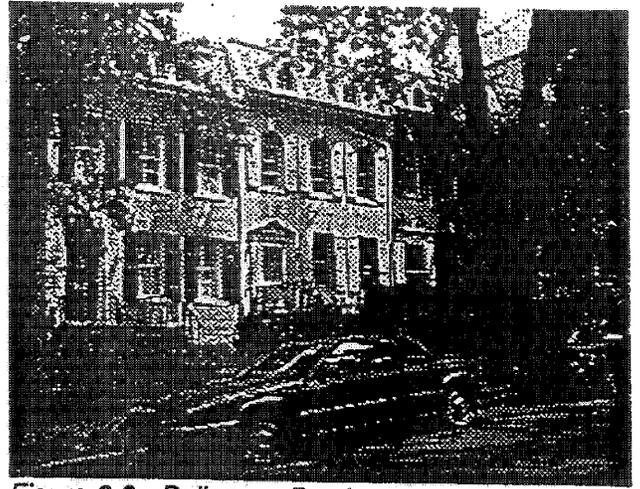


Figure 2-9: Ballenger Precinct Image



Figure 2-8: Eisenhower Precinct Image

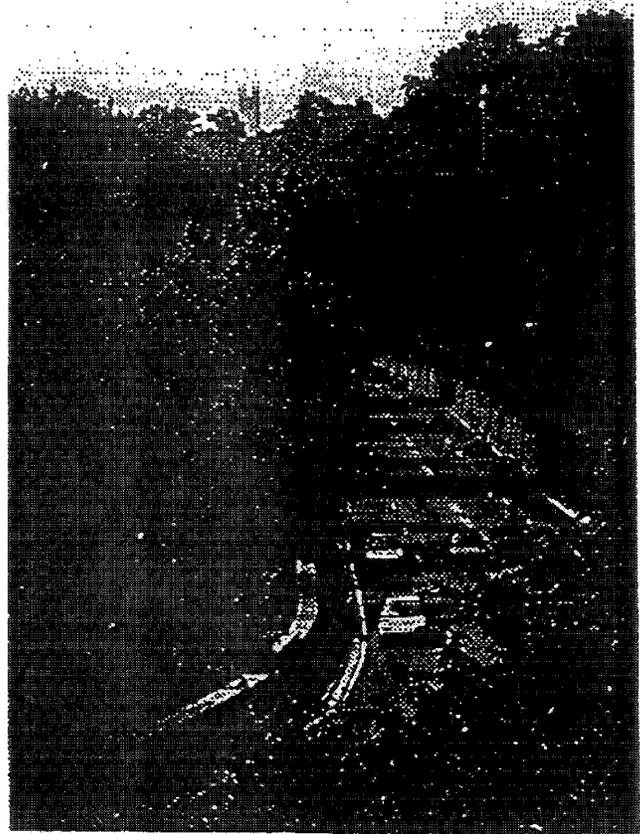


Figure 2-10: Holland Precinct Image



2.2 MAJOR OPEN SPACES

With the exception of Eisenhower Precinct, each precinct includes a major open space area which will have different relationships with the surrounding streetscapes. For example, Jamieson Precinct features an urban square with intense pedestrian activity from the surrounding streets; the streetscape allows free flowing access to and from the square. The major open space section of the Precinct Guidelines provides parcel owners with a concept and general design parameters for streetscape coordination. The major open space concept identifies the important open space features and streetscape elements such as street lights, sidewalk paving, street trees and other hardscape materials to unify the spaces with the streetscape. General design parameters are included to supplement the established precinct characters and outline the spatial organization, proportion, scale and other ordering principles, such as symmetry, hierarchy, rhythm and repetition. For example, Courthouse Square is a civic place characterized by its proximity to the Federal Courthouse occupying a symbolic central position in the precinct. The space is well defined by tall, massive buildings and is organized formally with the Courthouse streetscape.

The major open spaces will have varying degrees of influence on the surrounding streetscapes. Parcel owners shall use the open space concept and general design parameters as a basis for design of the streetscapes.

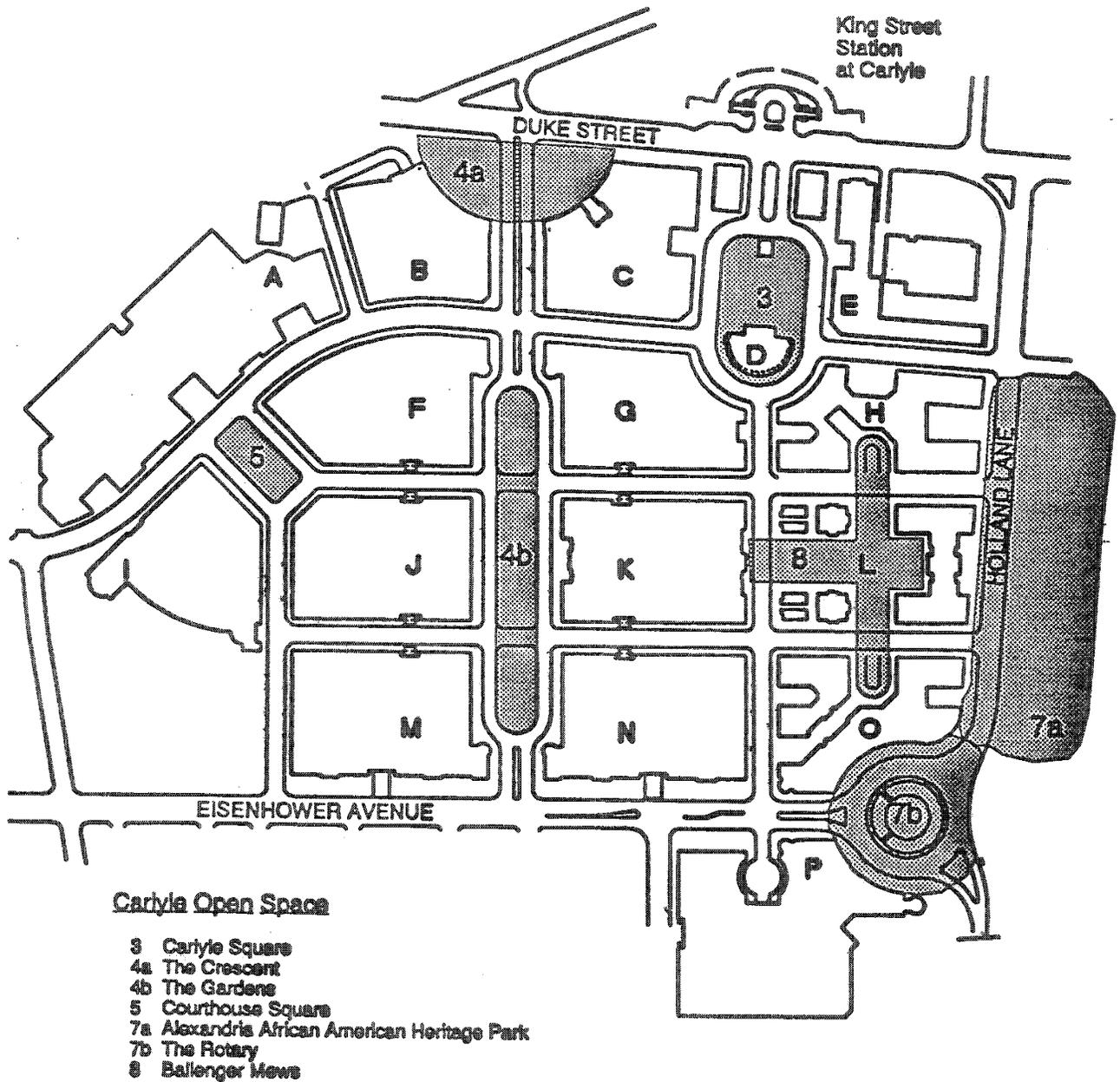


Figure 2-11: Open Spaces Location Plan



2.3 STREETScape DESIGN

Design of the Carlyle streetscape requires an understanding of the specific precinct - its context, use and imagery, and how the character should be expressed within the street. Streetscapes are defined by the street and the architecture that enclose them.

2.3.1 TYPICAL LAYOUT AND DIMENSIONS

The organization of streetscape elements occurs in identifiable zones based upon physical relationships between the building and the street. The zones are the Building Zone, the Pedestrian Zone and the Curb Zone.

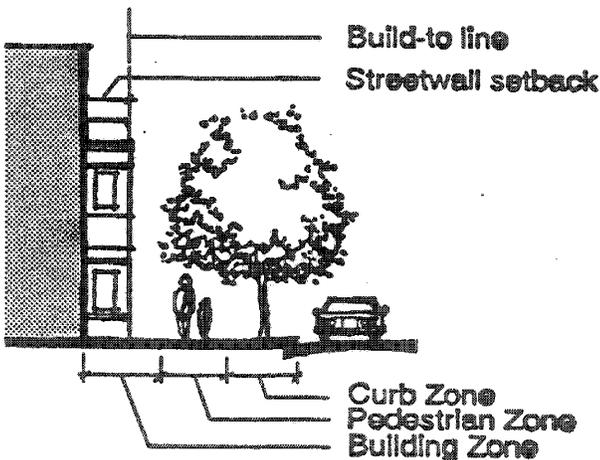


Figure 2-12: Streetscape Zones

BUILDING ZONE: The building zone is the zone of transition from the building streetwall to sidewalk. The zone may include plantings, architectural features, entrances, steps, awnings, seating, et cetera.

PEDESTRIAN ZONE: The pedestrian zone is the zone of concentrated pedestrian movement along a sidewalk, bound by the building zone on one side and the curb zone on the other. This zone is generally free of obstacles but may allow encroachments of planting, seating or kiosks.

CURB ZONE: The curb zone is the zone of transition from the sidewalk to the street - therefore containing the limits of the sidewalk by a curb. Most of the utilitarian fixtures of an urban street such as fire hydrants, street lights and street trees are located in this zone.

The three zones are not as clearly defined as the definitions might indicate. They are defined for purposes of indicating the location of all the elements in the streetscape; the zone edges will not necessarily correspond to the location of the right of way lines. The precincts shall cover each zone in detail; for more information refer to the appropriate chapter.

2.3.2 BUILDING ENTRANCES

Building entrances are significant architectural features that have aesthetic and functional purposes. The proportion, scale and architectural context of building entrances may identify uses and contribute to precinct character. The location and frequency of building entrances is the largest single impact on the building zone in the streetscape. There are five types of entrances as defined herein: lobby entrances, both major and minor, multiple entrances, minor entrances and garden court entrances.

MAJOR LOBBY ENTRANCE: A major lobby entrance is an entrance with an adjacent hall, foyer or waiting room typical of corporate offices and apartment buildings. It is large scale, one to two stories in height, visually prominent in the building facade and has greater architectural detailing than the other types of entrances. Major lobby entrances may incorporate special paving and lighting, and architectural projections such as a canopy or porte cochere. Elements of major lobby entrances may extend beyond the building zone and into the pedestrian zone up to the street right-of-way. For example, special paving, bollards, flags and banners may extend beyond the doors and along the sidewalk. Planting is required for articulation of the entrance and shall be a predominantly evergreen material. Special lights should be considered along with project or building signs. Steps and ramps are restricted to the building zone.

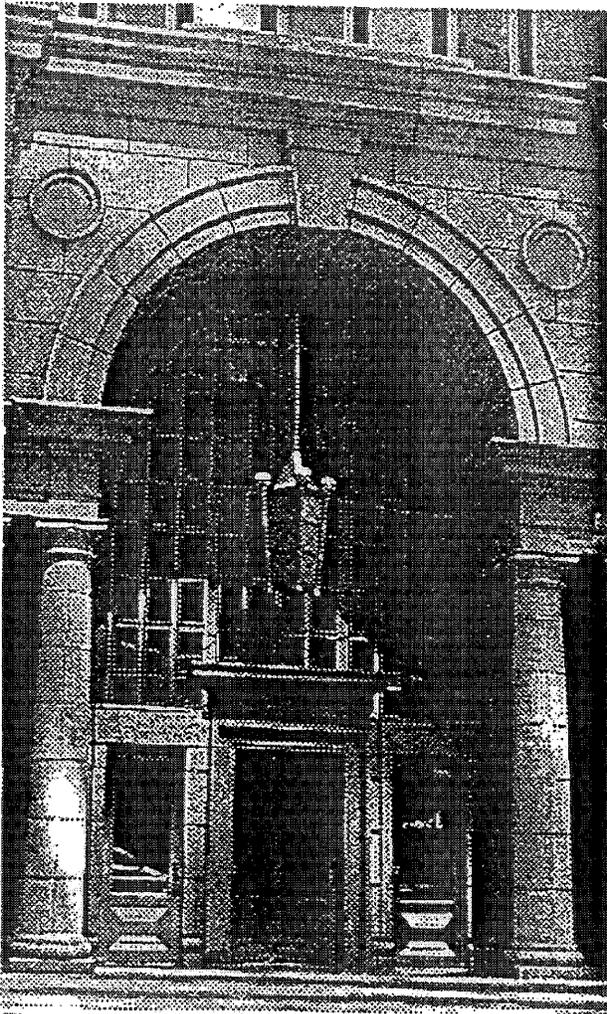


Figure 2-13: Major Lobby Entrance



Figure 2-14: Minor Lobby Entrance

MINOR LOBBY ENTRANCE: Minor lobby entrances have similar relationships to the streetscape as major lobby entrances. Minor lobby entrances are smaller scale, are usually one story in height and have simpler detailing. They are more integrated with and have less influence on the streetscape. Elements of minor lobby entrances should not extend beyond the building zone. Special paving and small architectural projections such as awnings may be incorporated into the design. Planting adjoining the entrance area is also required and shall have a minimum of fifty percent evergreen material. Special lights shall be low intensity and preferably incandescent. Steps and ramps are restricted to the building zone.

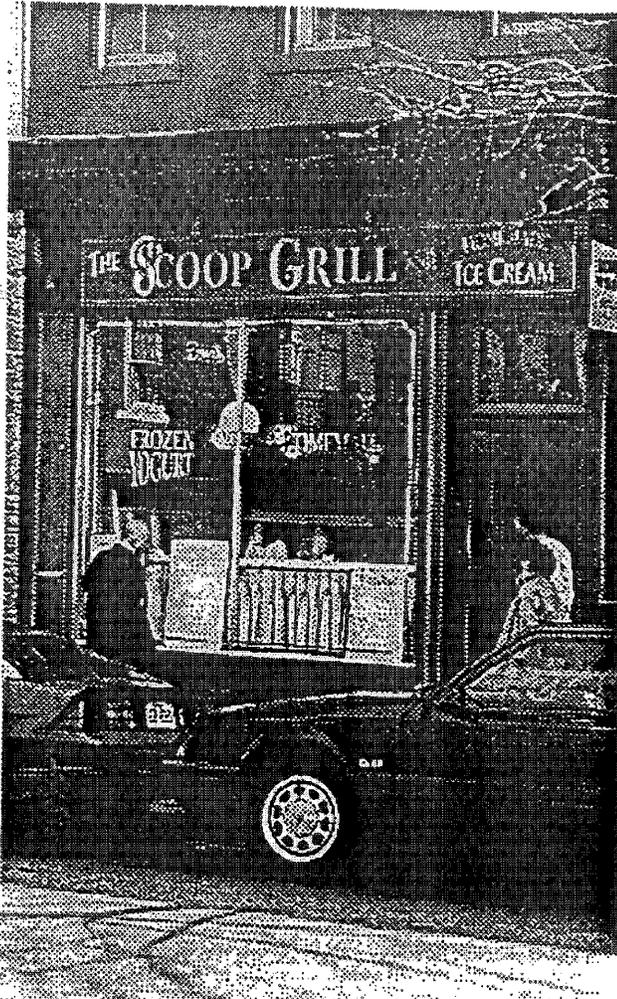


Figure 2-15: Multiple Entrance Retail

MULTIPLE ENTRANCE: A multiple entrance is a single entrance in a series of entrances; it may have a small foyer, generally serves a single tenant, and is typically residential, office condominium or one story retail. A multiple entrance is smaller than a minor lobby entrance. Although multiple entrances have similar emphasis, individual expression is encouraged by using different but related materials and streetscape elements.

MINOR ENTRANCE: A minor entrance is typically a secondary utilitarian entry such as an emergency exit or service door and incidental to the streetscape. A minor entrance will not have a foyer or special architectural detailing. No exterior elements associated with the other entrances would

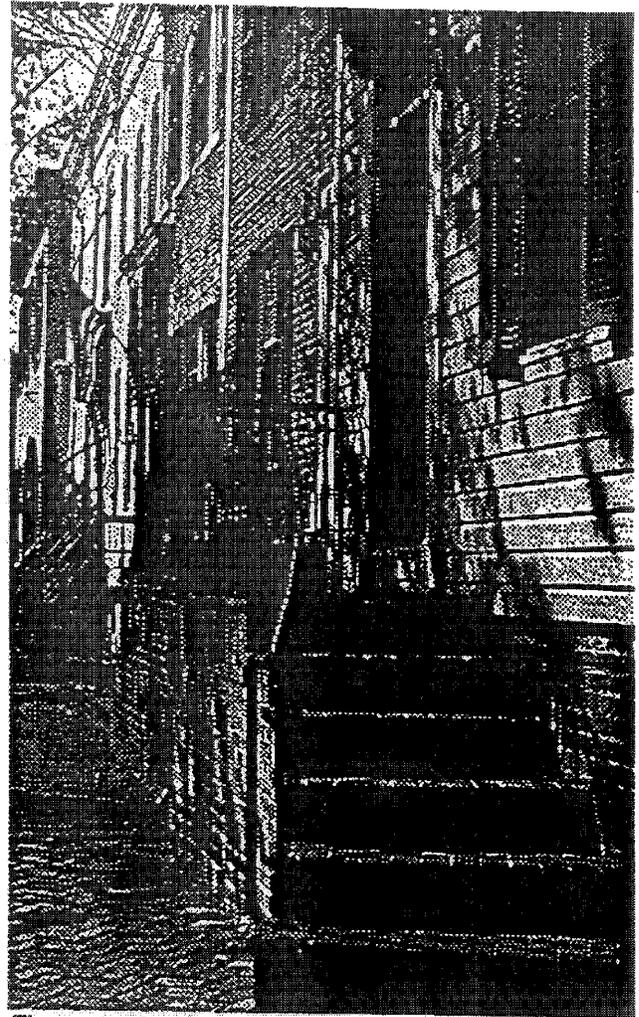


Figure 2-16: Multiple Entrance - Residential

occur at these entrances. Minor entrance doors shall not swing into the pedestrian zone. Where possible, minor entrances shall be recessed within the buildings or within planting beds in the building zone. No special paving is allowed; planting is encouraged but will not emphasize the doorway. Lighting shall be minimal, but will satisfy emergency access requirements. The standard brick sidewalk paving will extend to the threshold.

GARDEN COURT: A garden court is a special type of entrance for Carlyle used with residential buildings. It may be part of a major or minor lobby entrance. The garden court provides a semi-private front yard for the residents.

Special paving, planting, seating and special lighting may be combined in a unique design for a garden court. Garden courts may extend to the pedestrian zone.

2.3.3 VEHICULAR ACCESS ZONES

Vehicular access zones are driveways from streets to buildings that cross the pedestrian walkways. Therefore, careful attention must be given to provide safe crossing for pedestrians. The following section will discuss combinations of vehicular and pedestrian uses with their area of interaction and how they are accommodated by the streetscape.

PARKING AND SERVICE ENTRANCES: Parking and service entrances are generally located on the east-west streets in Carliye. They shall be constructed according to City Standards and meet the requirements of the Special Use Permit (Condition R-29).

Parking and service entrances shall be constructed to be flush with the sidewalk at the pedestrian zone. Brick pavers shall be mortared in place in a running bond pattern perpendicular to the street. The brick paver shall be the same paver as the adjoining sidewalk paving.

Streetscape elements such as trees, lights, planters and signs shall be located to allow safe vehicular movement and pedestrian crossing. Street Trees shall be located no closer than fifteen feet from the

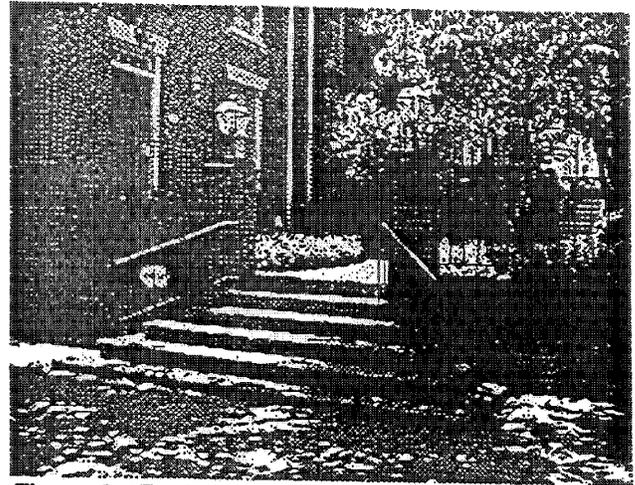


Figure 2-17: Garden Court

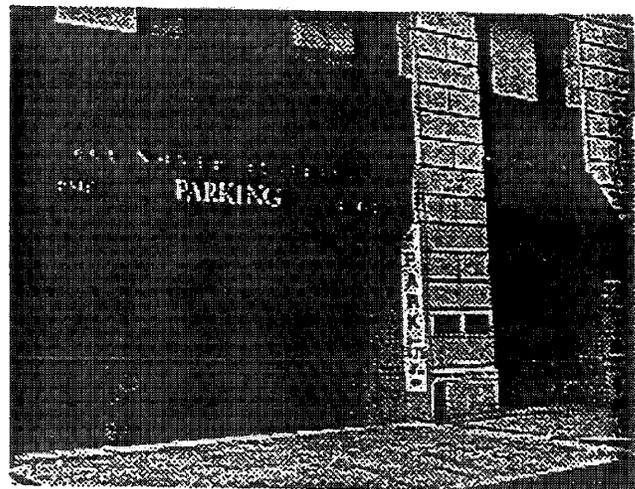


Figure 2-18: Parking Entrance

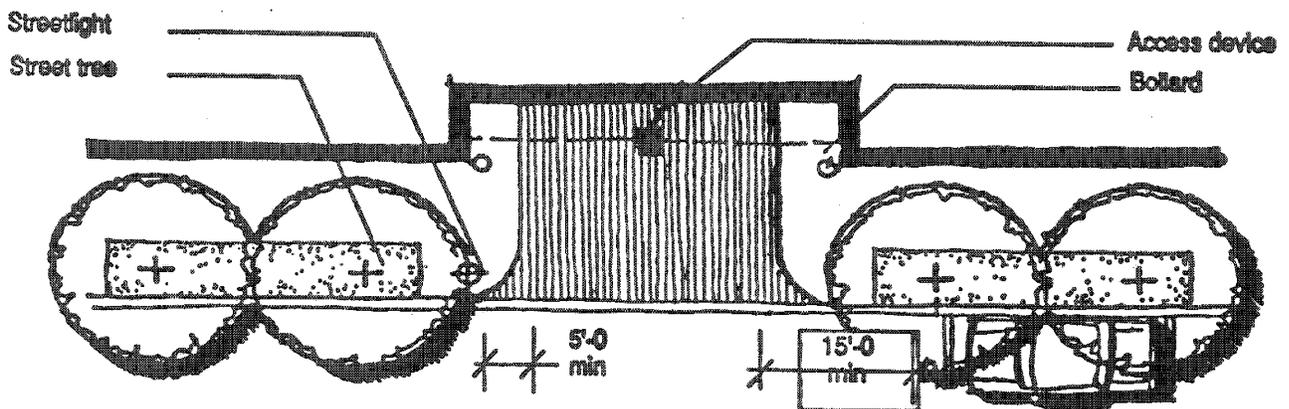


Figure 2-19: Parking/Service Entrance



entrance. Streetlights shall be located a minimum of five feet from the entrance, although where possible, they should be located nearby to provide additional light at the entrance. Electronic access devices shall be preferably located within the building or in the building zone. Bollards may be used to protect access devices and building corners. Information signs shall be placed on the building. Refer to the appropriate section on sidewalk paving, bollards and signs for further information.

DROP OFFS/LAY-BYS: Hotels, large office buildings and high rise apartments may incorporate dropoffs or lay-bys into the adjoining streetscape in conjunction with major lobby entrances if allowed by the approved development plans. Recessed building walls, porticos, porte cocheres and canopies may be incorporated into the architecture. The extension of special paving from a major lobby entrance to the dropoff is encouraged, provided the special paving does not extend into the right-of-way. Carlyle Development Corporation shall approve the use and location of all drop offs and lay-bys.

LOADING ZONES: On street loading zones will be subject to review by the City of Alexandria Director of Transportation and Environmental Services. The preferred location of these zones is on the east to west streets.

TAXI STANDS: Taxi stands may be located near uses that have need of taxi service such as a hotel or grocery store. After approval of the proposed

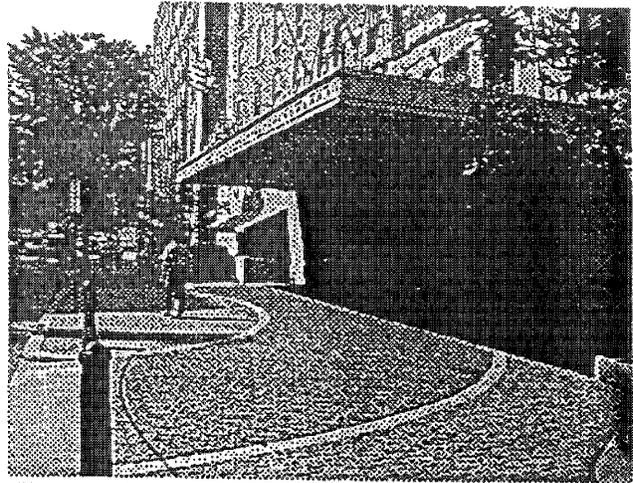
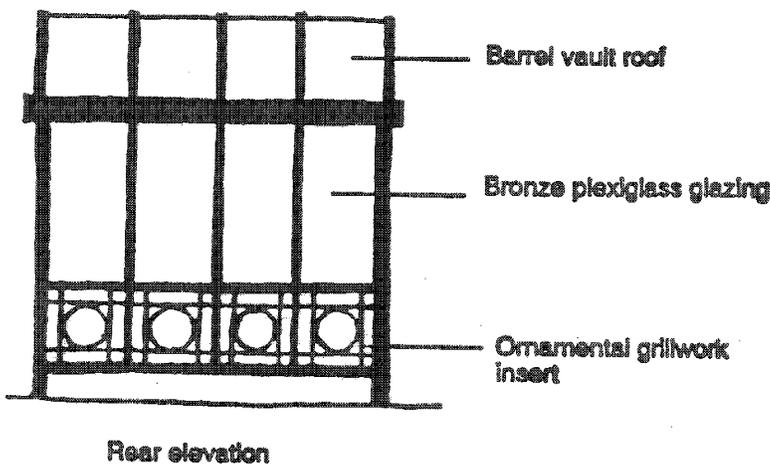


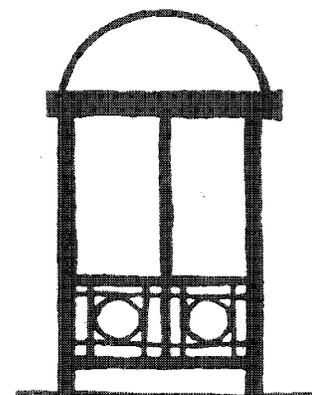
Figure 2-20: Drop Offs/Lay-Bys

location by Carlyle Development Corporation, the parcel owner must obtain approval from the City of Alexandria, Department of Transportation and Environmental Services.

BUS STOPS: A city operated shuttle service will operate through Carlyle between to the King Street and Eisenhower Avenue Metro Stations. Bus stops and shelter locations shall be determined by Carlyle Development Corporation, with the approval of the City of Alexandria, prior to final site plan. Bus stops shall be located in the curb zone on paved areas. A minimum of one bench and one trash can receptacle shall be provided per stop.



Rear elevation



Side elevation

Figure 2-21: Bus Shelter



As of January 6, 1994, bus shelter locations as proposed by Carlyle Development Corporation have not been accepted by the Alexandria Transit Company. When locations are finalized, they shall be included here and parcel owners will be required to install bus shelters as necessary.

Figure 2-22: Bus Stop and Shelter Locations

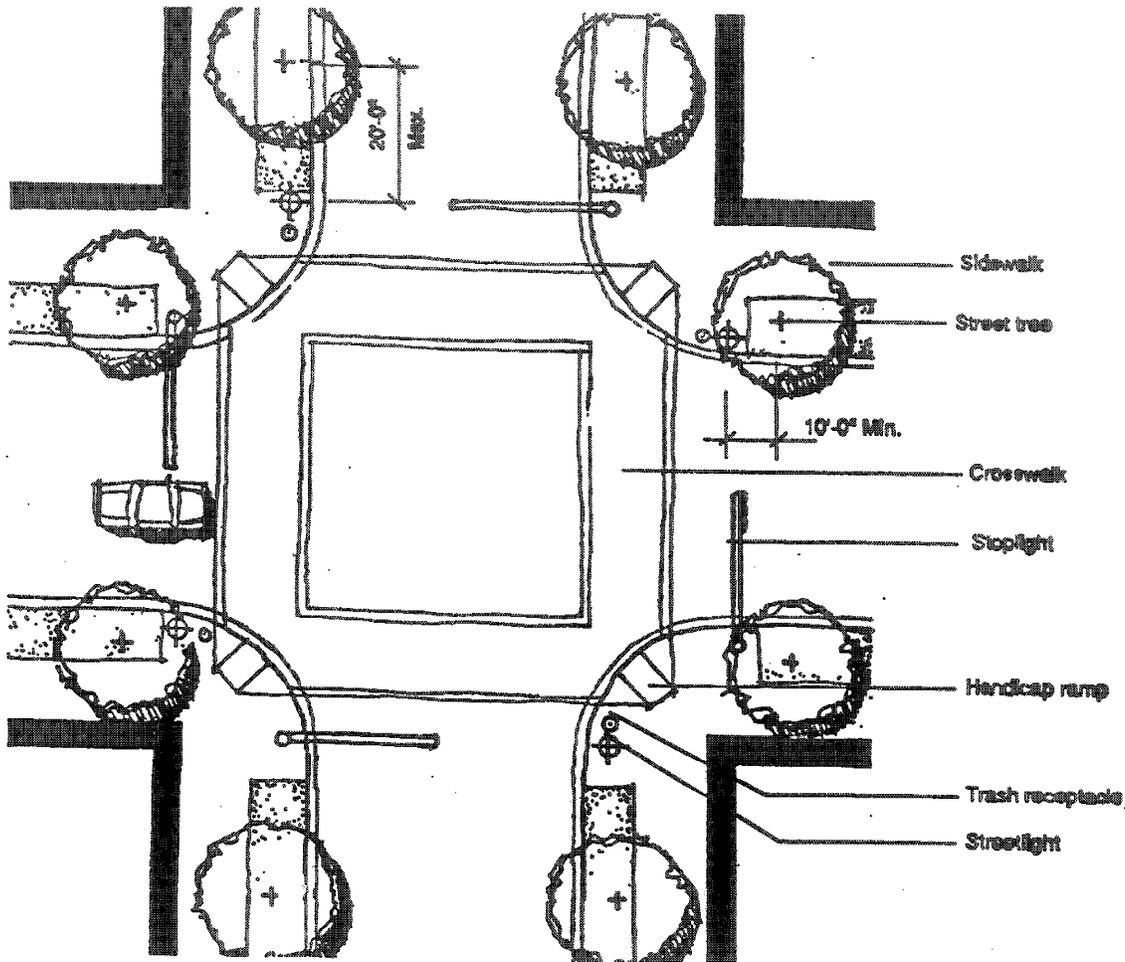


Figure 2-23: Typical Intersection

When it is determined that a bus shelter will be needed, the bus shelter shall be the standard custom design for Carlyle.

Bus shelters shall be the Modular Prefabricated Equipment Company, Inc., Jamaica, New York or equal approved by Carlyle Development Corporation. The shelters shall have bronze plexiglas glazing with a barrel vault roof and decorative metal grillwork. All metal shall be painted black.

2.3.4 INTERSECTIONS

Intersections serve as points of reference and transitional areas between precincts. Architecture, unique shops, clocks, banners, flags and other landmarks all contribute to the inherent character that differentiates one intersection from the next. Several streetscape elements are necessary at most intersections such as crosswalks, stop lights and signs, streetlights, trees, planters and paving. The following guidelines will help parcel owners coordinate these items at intersections.

CROSSWALKS: The typical crosswalk for Carlyle shall be the City of Alexandria standard crosswalk.



SIDEWALK PAVING: Sidewalk paving patterns shall be designed to turn the corner at intersections in a clean, simple manner. Handicap ramps shall be provided according to City standards; they shall be paved with the same brick pavers as the adjoining sidewalks.

STOPLIGHTS: The City of Alexandria Department of Transportation and Environmental Services and the Virginia Department of Transportation are responsible for determining final locations. Stoplights shall be mounted on the standard VDOT mast arms as installed elsewhere in Old Town Alexandria. The mast arms will be painted to match those in Old Town. Flags may be mounted to stoplight poles at intersections in certain locations; see Section 2.5.2 Street Fixtures, for this requirement.

STREETLIGHTS: One streetlight shall be provided at each corner as is typical along King Street in Old Town, Alexandria. Streetlights shall be located at the point of curvature or tangency of the curb at the intersection, in a manner that will not conflict with stoplight poles. The desired location for each streetlight is shown in the Typical Intersection figure.

STREET TREES: Street trees shall be located a minimum of ten feet and a maximum of twenty feet from the point of curvature or tangency of the curb at the intersection.

PLANTINGS AND STREET FURNITURE: Plantings and street furniture shall be designed and located in a manner that will not obstruct or hinder safe pedestrian or vehicular movement through the intersection. Appropriate clear sight lines must be maintained.



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2.4 HARDSCAPE

The hardscape or paving is one of the most visually dominant elements of the streetscape. Because of this, it is used to reinforce and differentiate the six precinct streetscapes of Carlyle. The hardscape includes the sidewalk paving, curbs, and street paving.

2.4.1 PAVING

STANDARD SIDEWALK PAVING: Standard sidewalk paving will be used throughout Carlyle, except as otherwise noted. It shall consist of a standard pattern and materials. The standard patterns are established on a precinct basis; refer to the precinct guidelines for the specific patterns to be used.

All sidewalks shall be paved with brick as specified by condition #R-28 of the special use permit. The brick shall be either the 4" x 8" King William Range brick paver by Baltimore Brick Company or the 4" x 8" Old Virginia #24 brick paver by Old Virginia Brick Company, Salem, Virginia or equal approved by Carlyle Development Corporation and the City of Alexandria. See the precinct guidelines for the specific paver to be used.

All brick sidewalk paving shall be dry laid with hand tight to meet the City of Alexandria Standards. A concrete base course shall be used to work with the street tree planting trough requirement in Section

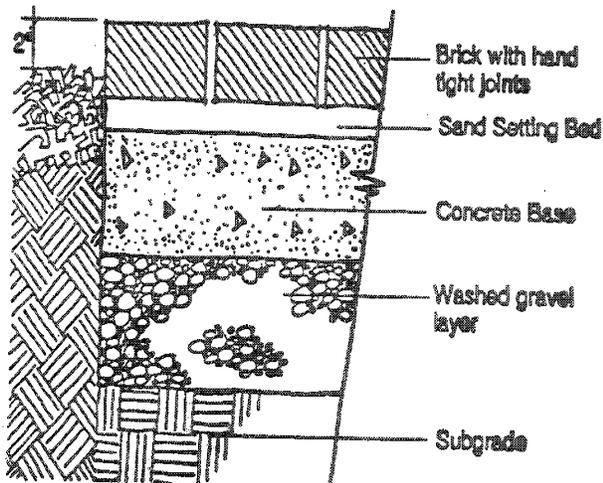


Figure 2-24: Brick Pavement Section

2.7.1 - Street Trees.

All sidewalk paving shall be installed by the parcel owner. The City of Alexandria will maintain the paving within the right-of-way; the sidewalk paving within the landscape easement will be maintained by the Carlyle Property Owners Association.

PAVEMENT EDGES: Pavement edges at street trees and planting areas in the building zone are detailed in several ways. The detailing responds to precinct specific design and may include a simple pavement edge, curbs or walls. The minimum requirement will be for a flush, clean edge of the sidewalk paving, unless other requirements are given in the precinct guidelines.

In some areas tree grates or curbs will be required. The grates are specified in the precinct guidelines. See Section 2.7.1 as well for further information on tree grates.

SPECIAL PAVING: Special sidewalk paving is allowed and encouraged in certain areas such as building entrances and within the open spaces. The design of the special paving shall be left to the parcel owner. The intent is to allow for individual expression of a particular building or place in the streetscape, however the special paving shall be carefully integrated with the standard streetscape paving. It shall also include brick that match or compliment the brick specified in the standard sidewalk paving for the precinct. Special sidewalk

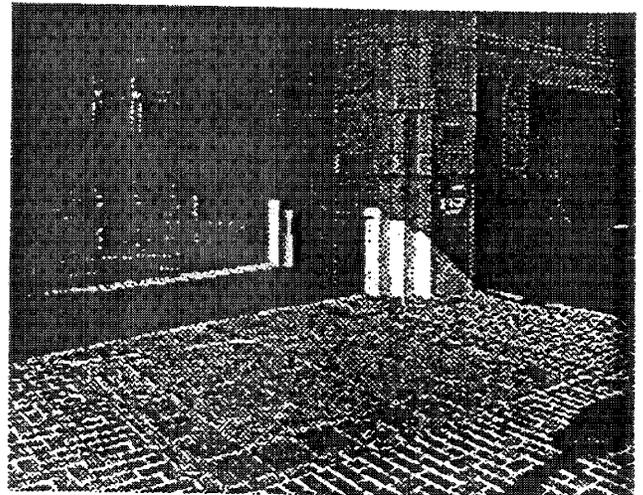


Figure 2-25: Special Paving



paving associated with buildings shall relate to the form, scale and materials of the architecture. Special paving areas shall not extend into the street right-of-way unless otherwise noted in the precinct guidelines. The design of all special paving areas must be approved by Carlyle Development Corporation. The maintenance for all special paving shall be the responsibility of the parcel owner.

STREET PAVING: All streets in Carlyle shall be paved with asphalt paving conforming to the requirements of the City of Alexandria. Concrete street paving will not be allowed.

2.4.2 CURBS

Six inch concrete curbs with gutters installed according to the City standards will be used throughout Carlyle. Curbs will be installed by Carlyle Development Corporation.

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2.5 STREET FURNITURE

This section of the Carlyle Streetscape Guidelines sets forth street furniture specifications and/or design approach. Each parcel owner is required to purchase and install these items in the streetscape consistent with the overall image of Carlyle. Specific requirements for location and quantities can be found in the precinct guidelines.

Consideration in the placement of street furnishings should be given to the appropriateness to context (ie., trash receptacle near take out restaurants), to minimizing obstruction of pedestrian traffic, to the quantity required to meet demand and achieve the desired effect (ie., Seating), and to minimize clutter and disassociated items (ie., cluster items in groups).

A matrix is provided in Appendix I to this chapter to summarize the furniture requirements and options for each precinct. It summarizes whether an item is required or optional in each streetscape zone. Additional requirements for location and quantities will be given here in the text. Locations may be specific or may simply indicate a zone. Appendix II in this chapter provides a reference list of the manufacturers or distributors for all furniture.

2.5.1 SEATING

Sidewalk seating opportunities are encouraged throughout Carlyle and shall be made available in both formal (bench, and table and chairs seating) and informal (seat walls, steps, ledges) seating types where the desired street character suggests it.

FORMAL SEATING - BENCHES: Benches used in Carlyle shall be the TimberForm Restoration Series (model number 2118, 2119 or 2123), manufactured by Columbia Cascade or equal approved by Carlyle Development Corporation. Bench seats shall be the standard Alaska yellow cedar; metal frames shall be finished in the standard black, powdercoat finish. The bench type within the series shall be chosen by each parcel owner, unless otherwise required in precinct guidelines.

Benches shall be permanently mounted to the sidewalk paving with tamper proof bolts. The

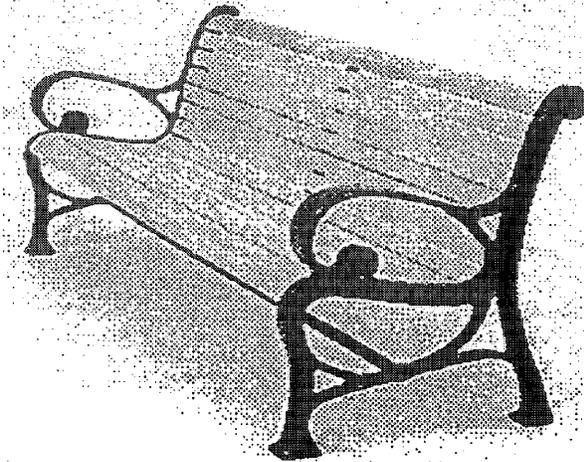


Figure 2-26: Model #2118-Restoration Series, Bench with Arms

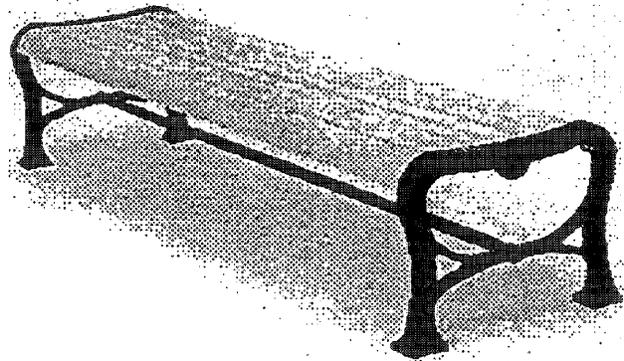


Figure 2-27: Model #2123, Restoration Series, Backless Bench

maintenance of all benches will be the responsibility of the individual parcel owners.

FORMAL SEATING - TABLES AND CHAIRS: The use of tables and chairs is encouraged in conjunction with cafes, restaurants, ice cream stores, coffee shops or any other particular use that could benefit from outdoor seating. All such outdoor areas require a special use permit and shall follow City regulations. Encroachment into the pedestrian zone shall be held to a minimum; seating/dining areas shall not occupy more than one third of the sidewalk width and shall not exceed the length of the store frontage.

All tables and chairs are the responsibility of the establishment owner. They shall be temporary and stored securely after hours. Their style and character should reflect the image of each particular establishment.

INFORMAL SEATING: Informal seating such as walls and steps is encouraged throughout Carlyle. Seatwalls primarily function as planters, however they shall allow for seating. Seat walls shall be at least sixteen inches high and a maximum of twenty-six inches high from finish grade to top of coping. Seatwall caps shall be a minimum of twelve inches wide and of material that matches the architecture and is compatible to sitting. Large prickly plant material, if any, shall be placed away from the seatwall edge.

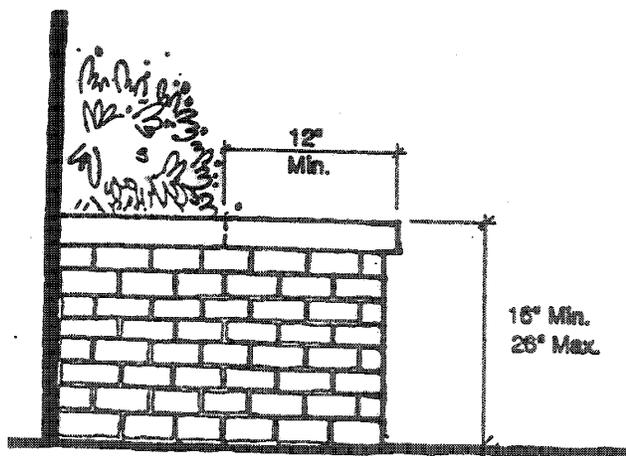


Figure 2-28: Seat Wall

Steps are a form of informal seating. Steps, when possible, should be wide enough that passage is not blocked by seating.

2.5.2 STREET FIXTURES

Street fixtures are additional street furnishings. The requirements and amount are based on the specific use in each precinct and user demand. In general, all furnishings should be located for pedestrian safety, handicap accessible, and meet all applicable standards and codes.

TRASH RECEPTACLES: The trash receptacle to be used throughout Carlyle shall be the Iron Site Bethesda Series receptacle (model S-42) with spun steel dome manufactured by Victor Stanley, Inc., Dunkirk, Maryland or equal approve by Carlyle Development Corporation. The trash receptacle shall be finished in a black, powdercoat finish.

Trash receptacles shall be permanently mounted to the paving surface and generally located in the curb zone. The quantity of receptacles installed shall respond directly to demand and frequency of trash collection. Minimum quantities are specified in the precinct guidelines. The receptacles will be maintained by the individual property owners. Collection of all refuse will be the responsibility of the City of Alexandria.

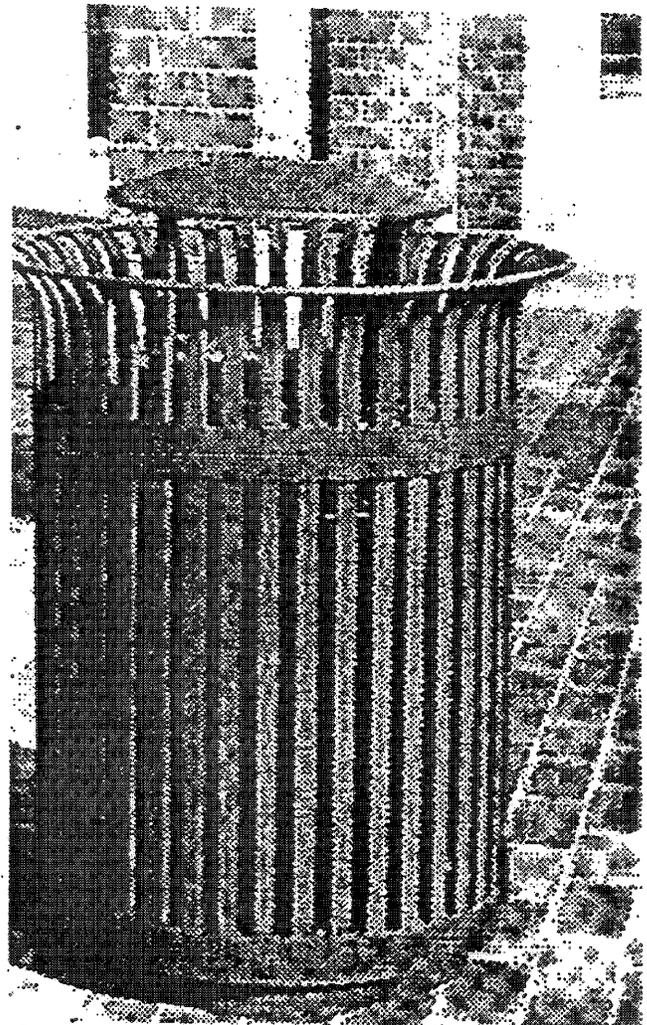


Figure 2-29: Trash Receptacle

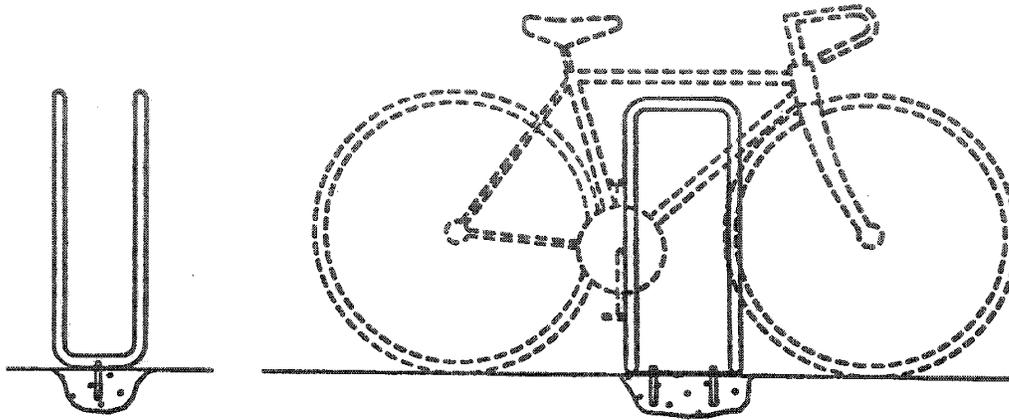


Figure 2-30: Bike Rack

BIKE RACKS: Bike racks shall be installed along the streetscape when determined to be needed. They shall be used to facilitate and encourage biking as an alternative means for transportation and to eliminate chaining of bikes to trees, streetlights and fences. If bike racks are needed, the Bicycle Sentry, Catalog #900 by Canterbury International, Los Angeles, California or equal approved by Carlyle Development Corporation, shall be used. Racks shall be finished in the standard black enamel powdercoat finish.

Bike racks should be placed in groups at convenient, visible, safe, well lit paved areas in the building zone or curb zone. Care must be taken to ensure that bikes and bike racks do not obstruct pedestrian movement. The bike racks will be maintained by the individual parcel owners.

In addition to the bike racks in the streetscape, parcel owners are required to provide bicycle parking within parking garages. Maintenance of these bike racks will be the responsibility of the parcel owner.

FLAGS AND BANNERS: Flags and banners are simple ways to animate and celebrate seasonal events and special activities in conjunction with planned events in Carlyle and Old Town.

Three flag mounting brackets matching the existing city brackets may be provided at the corner

streetlight or stoplight pole on the main streets in Carlyle. Flag brackets shall be finished to match the streetlight and stoplight poles. These brackets shall become the property of the City of Alexandria. Flags will be provided and changed by the City of Alexandria and/or the Carlyle Property Owners Association.

Flag poles may be used as an individual design element for specific buildings. Flag poles may be attached to building facades or placed in the ground. They shall be located within the landscape easement and shall be maintained by the parcel owner.

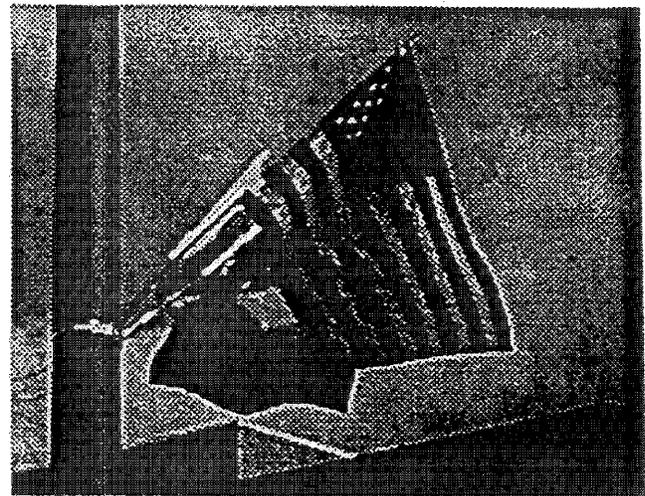


Figure 2-31: Flags at Corner Pole

Banners on buildings may be allowed in some precincts. These banners are required to convey a single design theme throughout that building's street frontage. The specific design of the building banners must be approved by Carlyle Development Corporation.

Break away mounting systems for all banners are needed to ensure pedestrian safety, due to wind loads and extreme weather conditions.

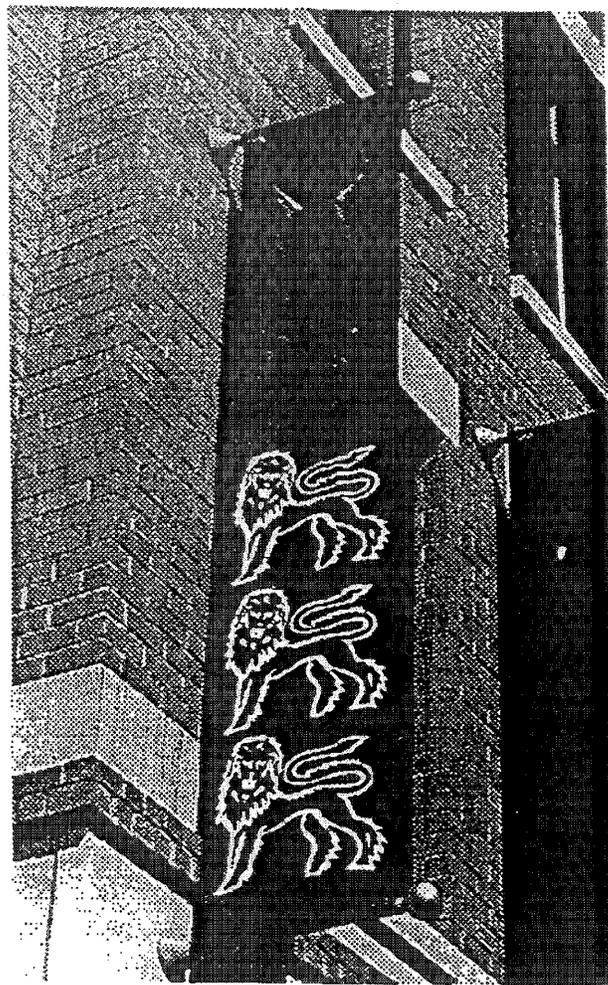


Figure 2-32: Banners



CLOCKS, ART AND OTHER SPECIAL FEATURES: Clocks, art and other special features add points of interest and landmarks to the urban streetscape. In order not to diminish their special effect, the location and frequency of clocks, art and other special features shall be controlled and approved by Carlyle Development Corporation. (i.e., The clock at King Street Station can be seen from many locations throughout Carlyle and is a strong visual tie to the Carlyle precinct, therefore further use of clocks in this precinct's streetscape may diminish the King Street Station clock's significant effect.) All clocks, art and other special features must occur in sidewalk easement and will be installed and maintained by the parcel owner.

FENCES, GATES, COLUMNS and WALLS: Fences, gates, columns and walls will be found throughout Carlyle. In some situations they will be an integral part of the architecture and shall compliment the materials, forms and colors used. Handrails and fences should also be coordinated. (i.e., garden court entrance to residential towers)

In other situations fences, columns, gates and walls will be used to identify and contain a particular outdoor use. (e.g., gardens at Dulany and Ballenger Mews) Here the material, form and color shall compliment the garden within and be inviting. Fences may be used with low walls and columns; the desired effect is a transparent barrier when used in public open space applications.

All fences, gates and columns shall occur within the landscape easement and within the described building zone and shall not encroach into the pedestrian zone. Fences shall be made of substantial materials such as iron, steel or wood in a sturdy design.

All fences, gates and columns shall be installed and maintained by the parcel owner.

Walls may be either retaining or freestanding, and faced in brick. The use of decorative details in walls such as water tables, recessed bricks, precast caps and other decorative brick is encouraged to provide interest at the streetscape level. The use of seat walls is also encouraged; see Section 2.5.1 - Informal Seating for more information.

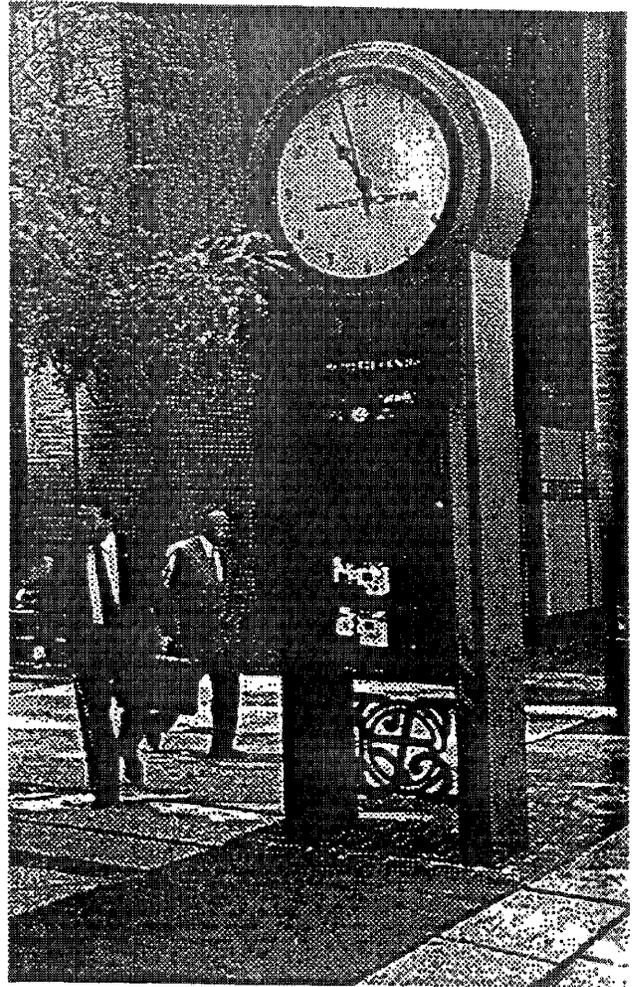


Figure 2-33: Clock

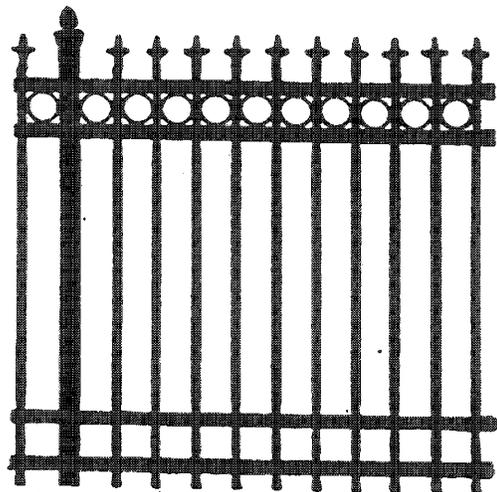


Figure 2-34: Residential Fence

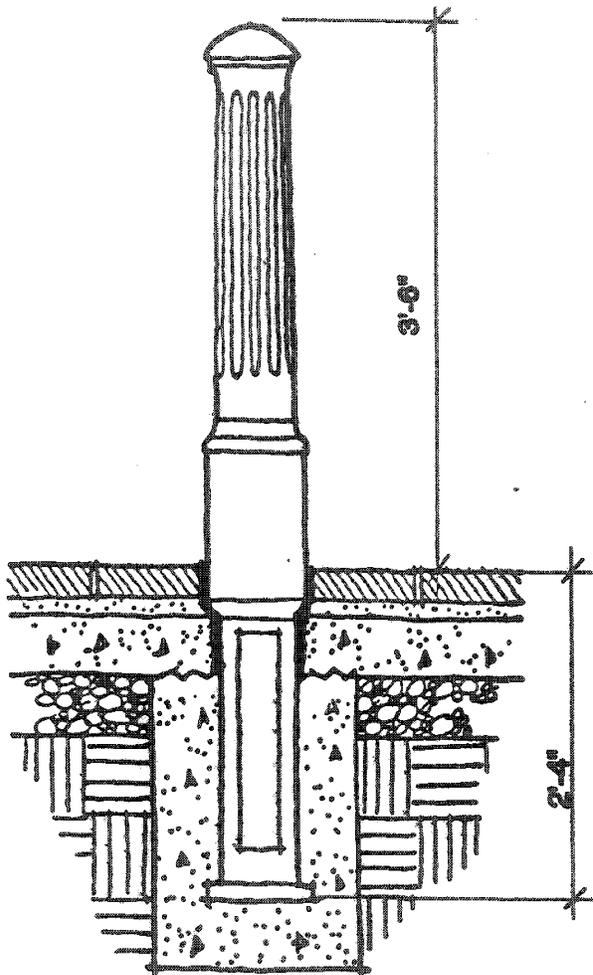


Figure 2-35: Decorative Bollard

BOLLARDS: Bollards will be used throughout Carlyle as traffic control and safety/protection devices.

Traffic control type bollards will generally occur in high visibility areas in the curb and pedestrian zones. They have been proposed at lay-bys, special entrances and in Jamieson Square. Bollards in these applications will be of a decorative type: the Princeton Embedded (direct burial) Cast Iron Bollard by Spring City Electrical Manufacturing Company. The bollard shall be finished in black to match the streetlight poles.

Less visible areas, such as building walls at service and parking entrances, that may require protection

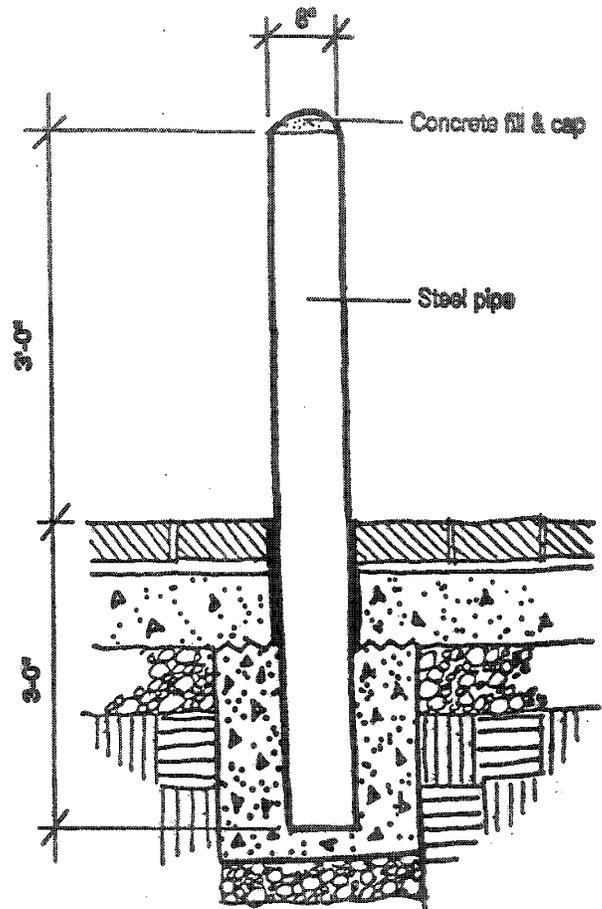


Figure 2-36: Simple Bollard

from automobiles may use a less decorative bollard type. This bollard shall be a simple round concrete filled metal post, painted to match the building architecture. The concrete cap shall be painted to match the metal post.

Bollards within the landscape easement shall be installed and maintained by the parcel owner. Bollards occurring within the right-of-way may become the City of Alexandria's responsibility if the City chooses to accept them.



DRINKING FOUNTAINS: Drinking fountains may be used in public open spaces and shall be custom designed to become an integral part of each open-space. The materials, finish and color of the fountain shall relate to its context. The responsibility of maintenance and providing water shall be determined during the design process for each specific area.

2.5.3 PUBLIC SERVICES

Many public services offered in an urban environment occur on the public streets. Each parcel owner must review the needs for his parcel for the following public services; final approval must be obtained from the appropriate agency.

No public service element shall obstruct vehicular sight triangles or safe pedestrian movement.

MAIL COLLECTION BOXES: Mail collection boxes will be placed according to a demand calculated by the United States Postal Service. Boxes will not be required near large office buildings or large residential buildings that have an interior mail room. The parcel owner shall review with the U.S. Postal Service his plans to determine the need for mail collection boxes. If the Postal Service determines that mail collection boxes are needed, they shall be installed according to Postal Service regulations and these guidelines.

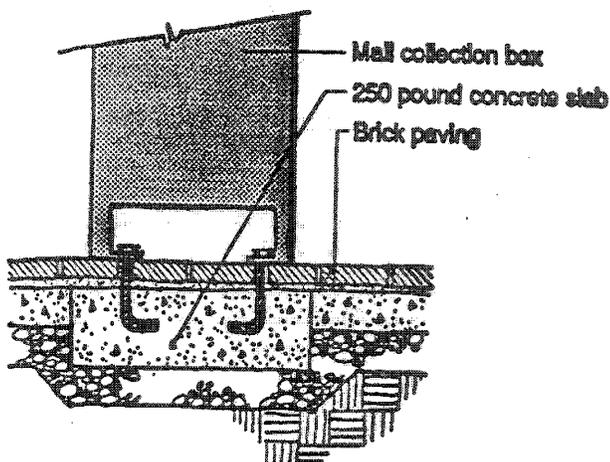


Figure 2-37: Mailbox Slab

Mail collection boxes shall be located in pavement areas in the curb zone; curb drop collection boxes are prohibited. The U.S. Postal Service prefers that the mail collection boxes are located near corners as opposed to mid-block locations. The U.S. Postal Service requires all mail collection boxes to be bolted to a 250 pound concrete slab. Parcel owners shall provide a poured in place concrete slab with bolts beneath the brick sidewalk paving meeting the specifications of the U.S. Postal Service. The parcel owner shall work directly with the U.S. Postal Service to finalize the location and installation of mail collection boxes.

Collection boxes for express mail services, such as Federal or Airborne Express, shall preferably be located in the building zone near major building entrances on pavement areas. Parcel owners will work with the express mail companies to locate the collection boxes integrally in the streetscape. Express Mail Collection boxes should be permanently bolted to the pavement similar to U.S. Mail Collection boxes.

NEWSPAPER VENDING MACHINES: Newspaper vending machines will be placed according to requests by parcel owners and demand determined by newspaper vending services. Parcel owners with retail components will establish newspaper sales within retail establishments or locate vending machines in interior foyers or interior parking garage access areas.

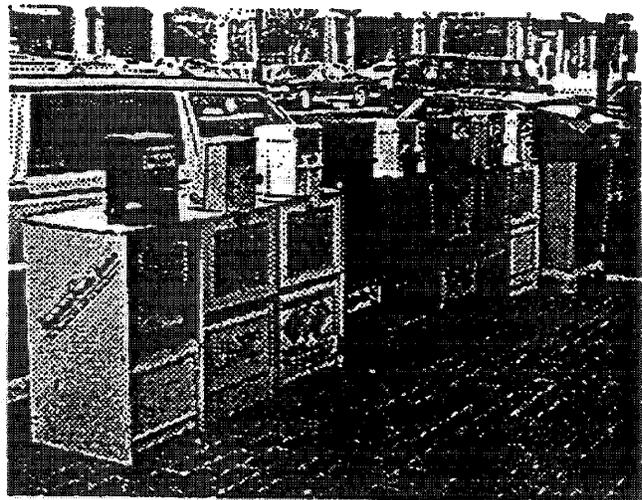


Figure 2-38: Newspaper Vending Machines



Newspaper vending services prefer to locate their vending machines near building entrances, bus stops and other places convenient to their customers. Parcel owners shall coordinate with the newspaper vending services to install the machines.

Newspaper vending machines shall be located against walls in the building zone or parallel or perpendicular to the curb in the curb zone on pavement areas. Multiple machines shall be clustered together in straight rows; not in haphazard clumps. The machines shall be bolted to the pavement for security; machines shall not be secured to streetlights, stoplight poles, trees or other street furniture with chains or cables.

The parcel owner shall inform Carlyle Development Corporation and Carlyle Property Owners Association of the location of the proposed machines. Vending machines remain the responsibility of the newspaper company for maintenance and supply. If the machine is neglected or improperly installed, the parcel owner shall contact the supplier and request its removal immediately.

PUBLIC TELEPHONES: Public telephones shall be located within major office and residential buildings in service areas adjacent to lobbies and in conjunction with proposed directories. If additional public telephones are needed, they shall be located within the building zone at street corners against building walls following the pattern in Old Town. The location shall be secure, visible and well lit.

The telephone enclosure for all exterior public phones shall be the Intermediate Enclosure Catalog number L35A/61 with the standard black anodized finish by Clark Specialty Company, Inc., Hammondspont, New York or equal approved by Carlyle Development Corporation. Public telephones shall be installed by a telephone company at the request of a parcel owner. Telephone service for public phones shall allow outgoing calls only. The telephones shall be maintained by the installing telephone company.

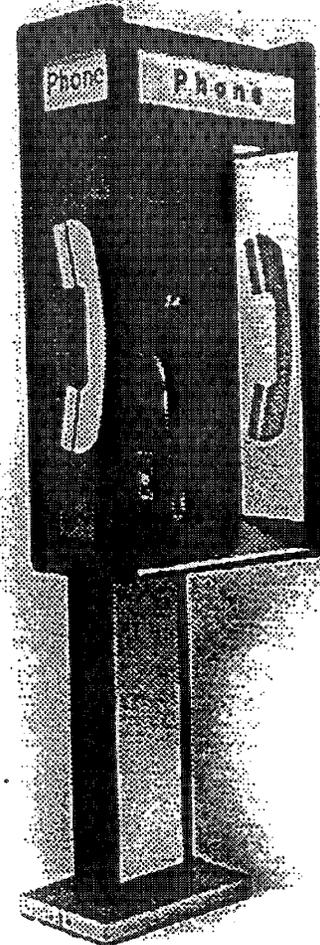


Figure 2-39: Public Telephone Enclosure



PARKING METERS: Parking meters if required by the City of Alexandria will be purchased by the by parcel owners. The City will then install them in cooperation with parcel owner. The City of Alexandria will collect the money from as well as maintain the meters. Meters will be mounted on two inch pipe, in pairs, and set back eighteen inches from face to curb to the face of meter.

VENDING MACHINES: Vending machines of all kinds except newspaper and automatic teller machines will not be permitted anywhere within the Carlyle streetscape.



2.6 LIGHTING

Lighting will greatly influence the sense of comfort and security. The willingness to linger in a place ultimately will influence its character and use. In Carlyle, a full range of lighting design opportunities exist for the streetscape including streetlights, specialty landscape lighting including uplighting, moonlighting, focal lighting and miniature lights, as well as building lighting of varying degrees.

2.6.1 STREETLIGHTS

A major element of the streetscape design is the streetlight, because of its frequency, repetition and impact at nighttime. The streetlight selected for

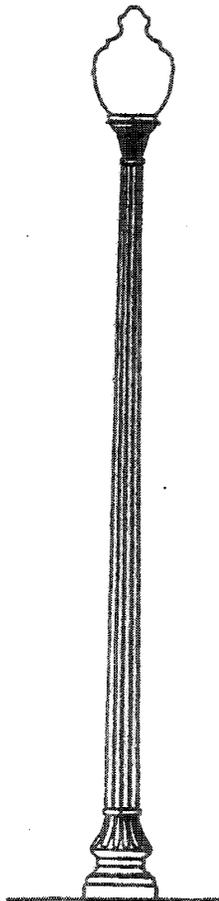


Figure 2-40: Single Luminaire Streetlight

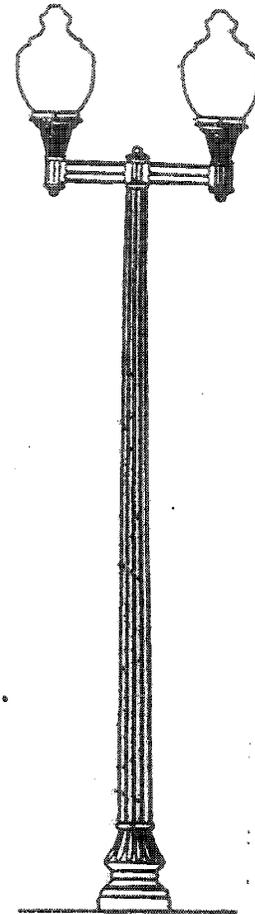


Figure 2-41: Double Luminaire Streetlights

Carlyle is the "Acorn" as shown on pages 22 and 23 of the Virginia Power Outdoor Lighting Manual. The "Acorn" streetlight is composed of the W.J. Whatley Washington series fiberglass pole and General Electric Edison III luminaire. A Hadco Victorian III series refractive globe will be substituted for the Edison III luminaire's standard polycarbonate globe and type V refractor. This streetlight or equal approved by Carlyle Development Corporation and Virginia Power will be used throughout Carlyle except along Duke Street where the standard City of Alexandria, "Gadsby" streetlight shall be used, matching King Street Station.

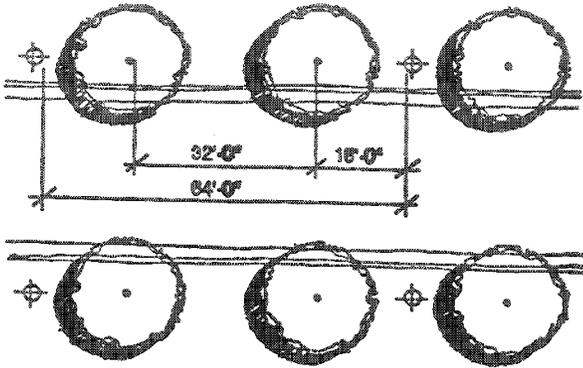


Figure 2-42: Paired Street Lights

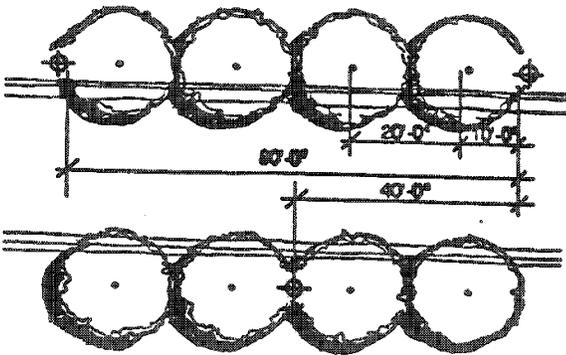


Figure 2-43: Staggered Streetlights

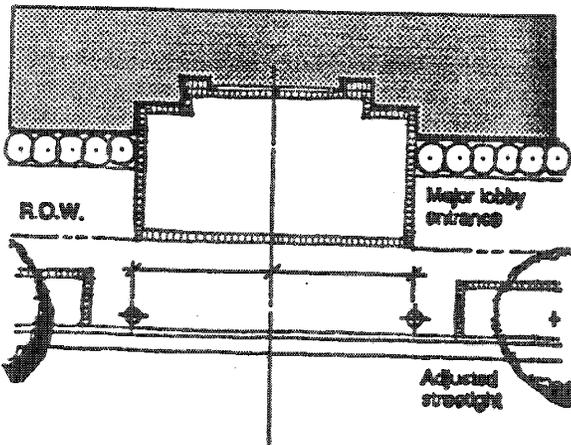


Figure 2-44: Coordinated Streetlights

The light source shall be a high pressure sodium lamp at an appropriate wattage to meet the required lighting levels.

Generally, a single luminaire mounted to Washington Series 405 fluted tapered fiberglass composite pole as manufactured by W.J. Whatley, Commerce City, Colorado will be used. The pole shall be fourteen feet in height above grade. In some areas, double luminaires shall be used mounted on the fourteen foot Washington series 405 pole with a thirty-six inch Celtic series arm. The locations of the double luminaires is specified in the precinct chapters and shown on the Carlyle Infrastructure Plan.

All streetlight luminaires, poles and accessories shall be finished in the standard black finish.

The Carlyle Infrastructure Plans show the locations of the streetlights. Streetlights are generally spaced at sixty-four feet on center, paired across the street or eighty feet on center in a staggered pattern. They are centered on a line two feet, nine inches from the face of curb. For good light distribution, streetlights shall typically be located halfway between street trees. When new streetlights are installed on the opposite side of a street where streetlights already exist, the placement of the new streetlights shall be coordinated to create the paired or staggered effect specified. Special spacing requirements may be required for certain areas; see the precinct chapters and the Carlyle Infrastructure Plans.

Placement and spacing of the streetlights may need to be adjusted to meet other requirements in these guidelines. These other requirements include location at intersections (see Section 2.3.4) and at Parking and Service Entrances (see Section 2.3.3). Special attention should also be given to the location of streetlights at major lobby entrances. The streetlight locations shall be coordinated with the entrance design. At all other entrances, the normal spacing of streetlights as shown on the Carlyle Infrastructure Plans shall prevail.

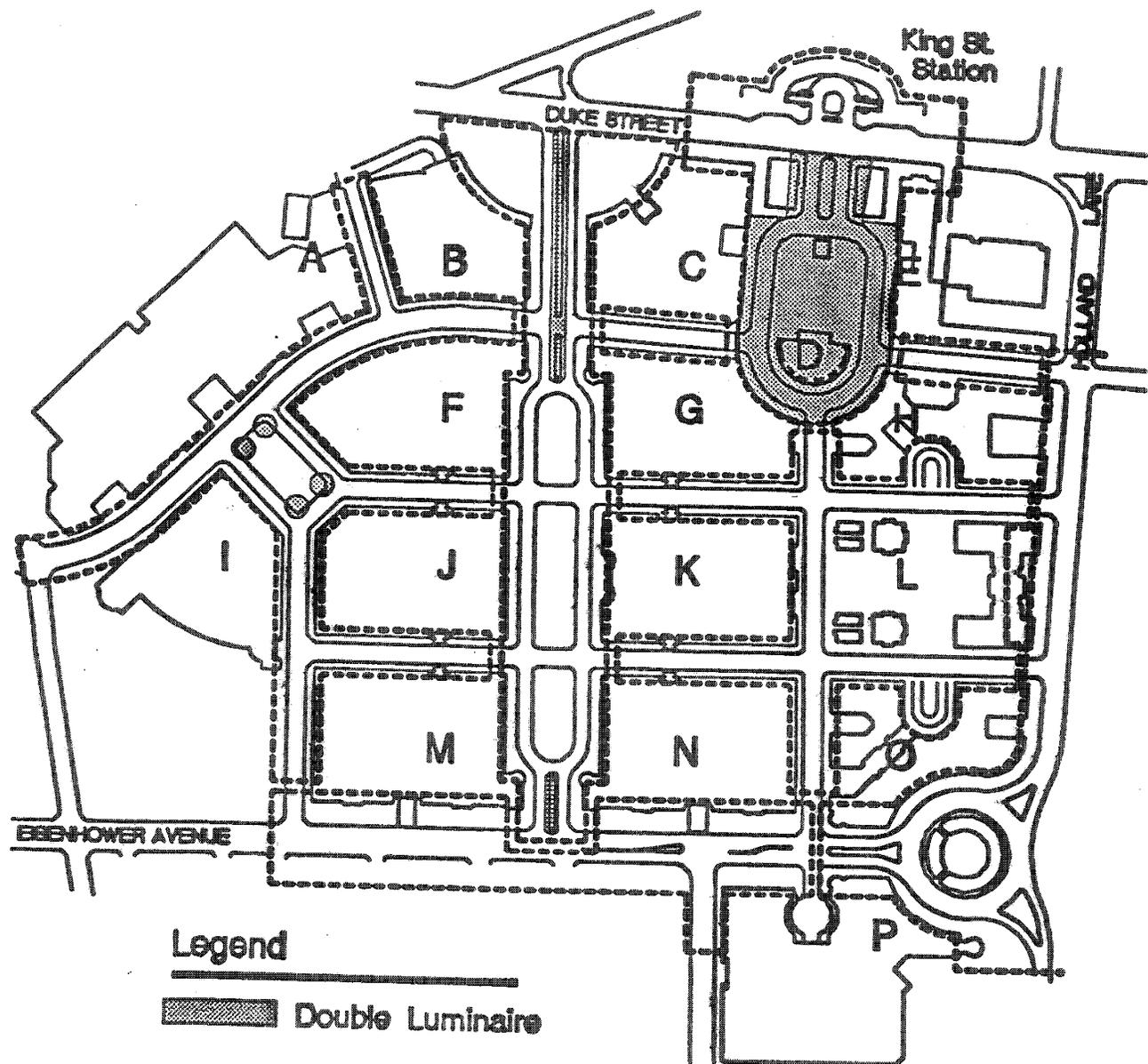


Figure 2-45: Double Luminaire Locations



2.6.2 SPECIALTY LANDSCAPE LIGHTING

Specialty lighting to highlight landscape features in the streetscape is allowed and shall be installed at the discretion of the parcel owner. The use of specialty lighting is also encouraged in the open spaces.

Care must be exercised in the design of landscape lighting to avoid excessive light and glare. Consideration shall also be given to the type of light source, its color rendition, and the coordination of that color with adjacent light sources. The streetlights which have metal halide lamps will be the predominant light source in the streetscape. The effect of color rendition on the colors of the materials and plants should also be considered.

Light fixtures shall be simple and unobtrusive in design. The fixture and its placement shall not intrude into the streetscape design so as to draw attention to itself, unless the prominence is a deliberate feature of the landscape design.

Landscape lighting includes uplighting, moonlighting, focal lighting and miniature lights.

UPLIGHTS: Uplighting is a dramatic way to highlight the form of a plant, sculpture or other landscape feature. Its success depends on the creation of contrasting light and shadow areas. Uplighting may be used for plantings within the building zone and other landscape features. Except as noted for specific areas, the uplighting of street trees is not permitted.

FOCAL LIGHTING: Focal lighting is used to spotlight a specific plant, planting or other landscape feature in a manner that provides additional emphasis not achieved through even light distribution.

MINIATURE LIGHTS: Miniature lights similar to Christmas lights lend a festive quality to a place at night. They are often used to highlight uses with an evening orientation such as nightclubs and restaurants.

The design of permanent installations of miniature lights must be approved by Carlyle Development Corporation. Miniature lights shall be white in color only; flashing, race or twinkling lights are not permitted.

2.6.3 BUILDING LIGHTING

The specific design for building lighting is at the discretion of the parcel owner, however these guidelines may encourage special building lighting in certain areas. Building lighting includes building mounted fixtures and facade lighting.

BUILDING MOUNTED FIXTURES: Building mounted fixtures such as sconce lights shall be appropriate in character and scale to the building



Figure 2-46: Building Mounted Light



architecture and the adjoining streetscape. Fixtures may be used to give emphasis to major lobby entrances and lesser emphasis to minor lobby entrances. Appropriate smaller scale fixtures are encouraged for multiple entrances. Consideration should be given to the additional light provided to the adjoining streetscape by these fixtures.

Building mounted "wallpack" flood lights which wash an area with bright light are not permitted on the streetscape facades of buildings.

FACADE LIGHTING: Facade lighting is washing the facade of a building with light, providing emphasis for the building at night. Facade lighting should enhance the massing and detail of a building through the use of light and shadow. Even lighting of all facade areas is not permitted.

Depending on the location and prominence of the building, portions or all of a building facade may be lit. These guidelines will suggest where this type of building lighting is appropriate. The amount and location of all facade lighting is subject to the approval of Carlyle Development Corporation.



2.7 PLANTING

Well designed, properly installed plantings will be a part of the Carlyle Streetscape. Some of the plantings will be specified by these guidelines; other plantings in the streetscape will be allowed or required but the specific design will be left to the individual parcel owner. Plantings in Carlyle are intended to create a lush, green atmosphere similar to Old Town Alexandria. Variety in plantings is encouraged; massive monocultural plantings are discouraged.

All plants installed in Carlyle shall meet the *American Standard for Nursery Stock*, latest edition as published by the American Association of Nurserymen. Plants shall be sized and spaced to provide adequate impact upon installation, meaning the plantings should "fill-in" in one to two years. All plants shall be installed according to the *Landscape Specification Guidelines for the Baltimore-Washington Areas* unless equivalent specifications are prepared. To maintain soil moisture and reduce weed growth, all plantings shall be mulched using double shredded hardwood bark mulch.

All plantings shall be installed by the parcel owner; they will be maintained by the parcel owner or the Carlyle Property Owners Association depending on the location and type of planting. Generally, plantings associated with a specific building shall be maintained by the parcel owner while plantings within the public open spaces (Carlyle Square, The Crescent, The Gardens, Courthouse Square, Alexandria African American Heritage Park, The Rotary and The Mews) will be maintained by the Carlyle Property Owners Association. See Special Use Permit Condition #R-15 for more information.

2.7.1 STREET TREES

Street trees are required along all streets in Carlyle. To ensure uniformity and continuity of design, the species and cultivars for the street trees shall be as designated in these guidelines. Refer to the precinct guidelines for more information.

Unless otherwise required street trees shall be installed at a four inch minimum caliper. Street trees shall generally be spaced at twenty feet on center, except along Jamieson Avenue, around Carlyle Square and around Dulany Gardens. See the appropriate precinct chapter for the particular spacings in these areas. The location of major lobby entrances, intersections and parking/service entrances affects the spacing of street trees.

Spacing of the street trees may be varied slightly to accommodate these breaks in the street. Refer to the sections on Entrances (2.3.2), Intersections (2.3.4) and Parking/Service Entrances (2.3.3) for information regarding the location of street trees in these areas.

All street trees shall be planted in a continuous planting trough along the curb as required by S.U.P Condition #R-49, with aeration, drainage, and irrigation systems. This trough must be large enough to provide sufficient arable soil volume to support adequate moisture for the tree. A planting trough for a single tree shall contain a minimum of 300 cubic feet of soil. When there are multiple trees planted in one trough, a minimum of 240 cubic feet of soil shall be provided per tree. These amounts are

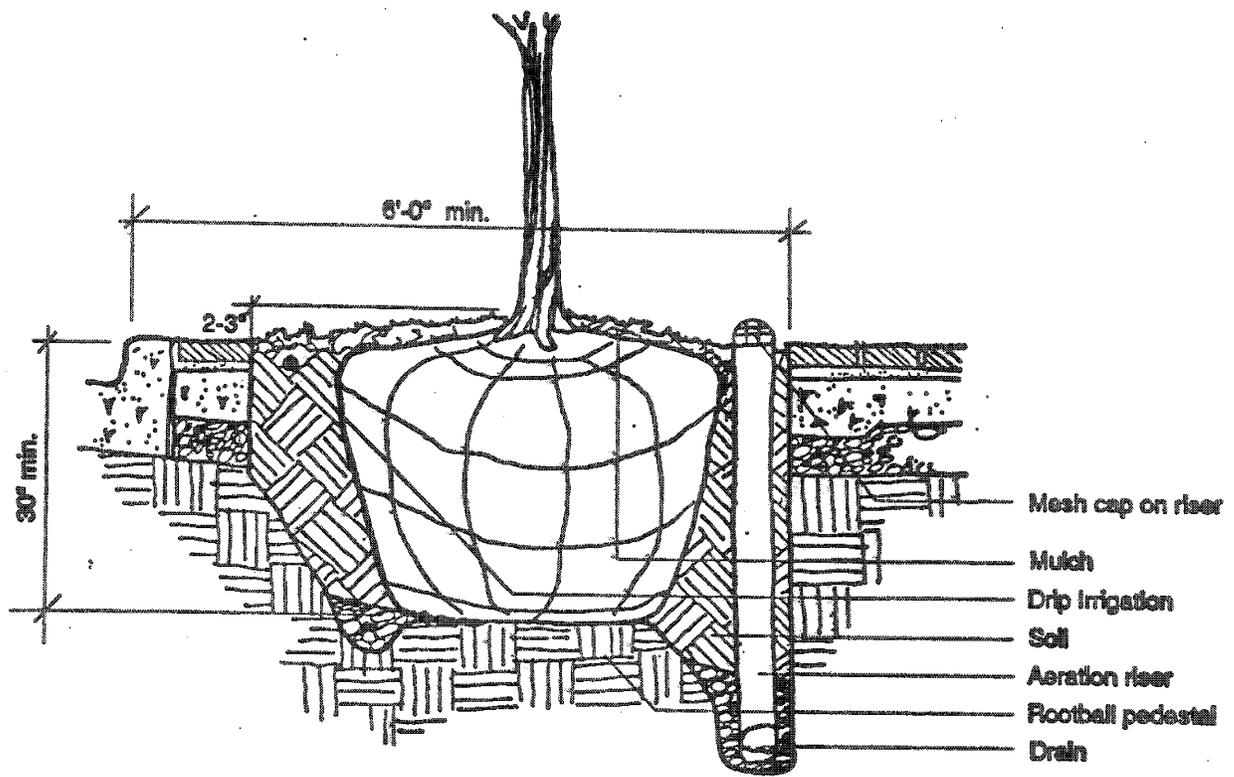


Figure 2-47: Planting Trough at Tree

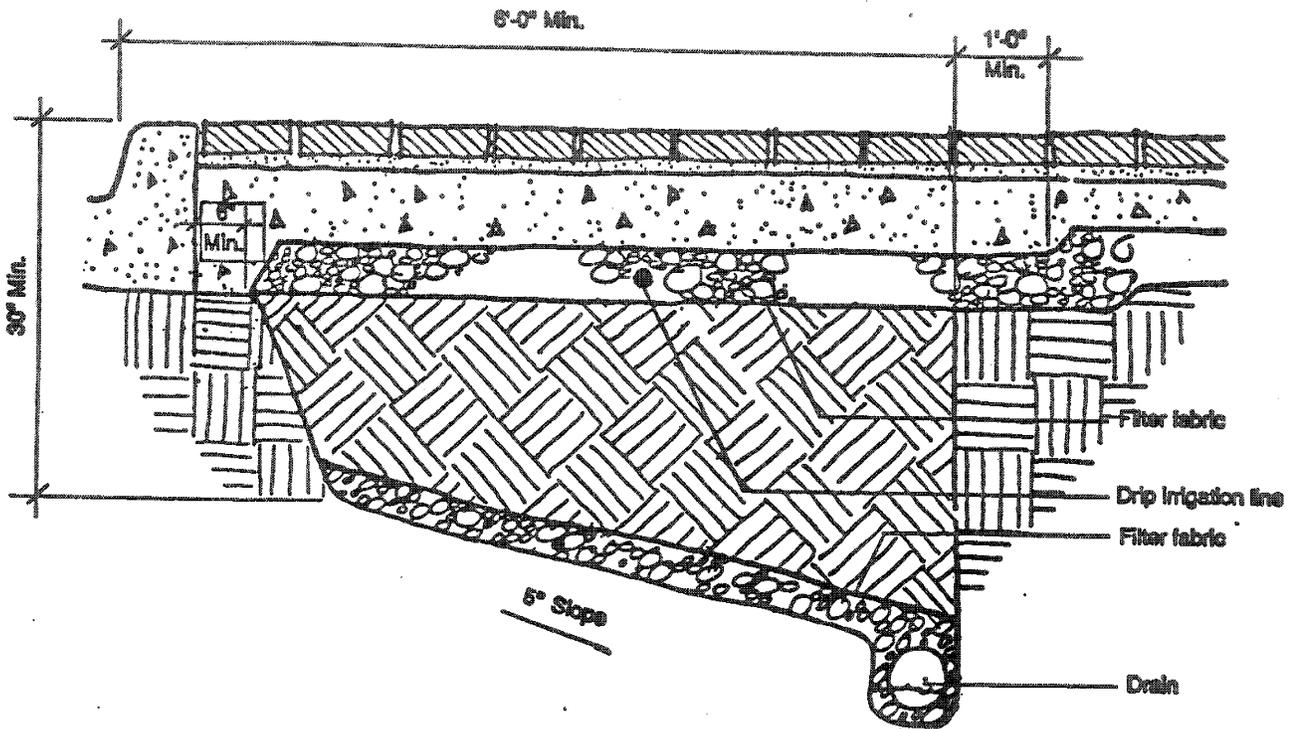


Figure 2-48: Planting Trough Between Trees

minimums; where it is possible, more soil volume shall be provided. Troughs shall be a minimum of thirty inches deep and six feet wide from the face of curb. The maximum depth of the trough shall be no deeper than is necessary to install the tree given the size of its rootball. Special care shall be taken to not disturb curb and gutter when installing planting troughs.

Tree well openings in the sidewalk shall be sized as required in each precinct. Between tree well openings, the sidewalk shall be suspended over the tree planting trough. The planting soil and aeration, irrigation and drainage systems for the trough must be in place prior to construction of the sidewalk. Every effort shall be made to not compact the

planting soil during sidewalk construction. The typical sidewalk cross-section may need to be modified to provide adequate structural support for the sidewalk and road. Planting troughs are not required to continue beneath parking or service entrances.

Drainage shall be provided for planting troughs with an underdrain. This drain shall outfall to the storm drainage system. Adequate aeration of the planting trough shall also be provided. One riser from the underdrain will be provided at each tree to allow air down into the planting trough. The riser shall be capped with an open cap to prevent trash from accumulating in it while maintaining air circulation.



In addition, washed stone without fines shall be provided under the sidewalks to allow air to circulate at the surface of the planting trough. Aeration sheets may also be provided.

An automatic irrigation system shall be provided for the tree planting troughs. This system shall consist of a drip irrigation line placed at the top of the planting trough and shall be connected to the building's water system.

Electrical outlets may be required in certain areas at each tree well for seasonal lighting displays. Specific areas will be addressed by the precinct guidelines.

Street trees will be installed centered in the tree well opening at two to three inches higher than the finish grade of the tree well. Root balls shall rest on undisturbed sub-grade or adequately compacted fill to prevent settling. Soil shall be kept a minimum of two inches below the pavement edge to prevent it from spilling onto the sidewalk. Planting soil shall be a fertile, well granulated, medium sandy clay loam.

In certain areas of some precincts, tree grates are required. These grates shall be installed set flush with the pavement surface in frames. Trees shall be installed to allow a two inch gravel mulch to be placed under the grate.

All trees shall be guyed for the first year following installation. Three stainless steel or hot dipped

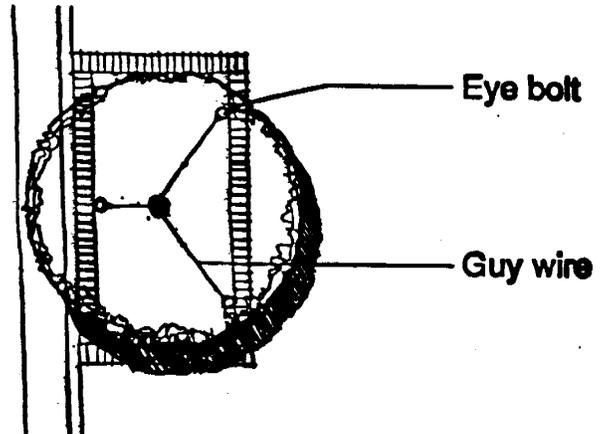


Figure 2-50: Eye Bolts in Triangular Pattern

galvanized eye bolts shall be set in the concrete pavement base at the edges of the tree well in a triangular pattern to be used for guying the tree. Eye bolts shall be below the soil surface. Guy wires shall be promptly removed one year after the installation of the tree. Eye bolts should remain in place in case a tree should need to be replaced in the future. When trees are planted in lawn areas, they shall be staked or guyed according to the *Landscape Specification Guidelines for the Baltimore-Washington Area*.

Street trees shall be installed by the parcel owner and maintained by the Carlyle Property Owners Association. The City of Alexandria is responsible for the replacement of street trees within the public right-of-way (see S.U.P. Condition #R-15.)

2.7.2 PLANTING BEDS AND PLANTERS

Other plantings may be required or allowed especially in the building zone. The design of these plantings is left to the parcel owner, however, they should always include an evergreen component. Plantings should be appropriate to the character of each precinct. Specific requirements for the quantity of planting are on a precinct basis; see each precinct chapter for more information.

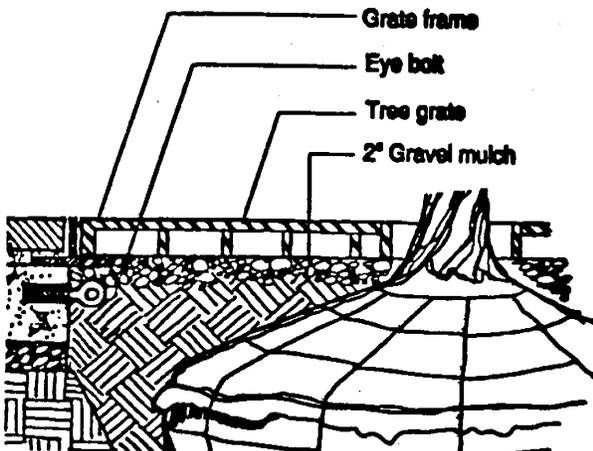


Figure 2-49: Gravel Under Grate

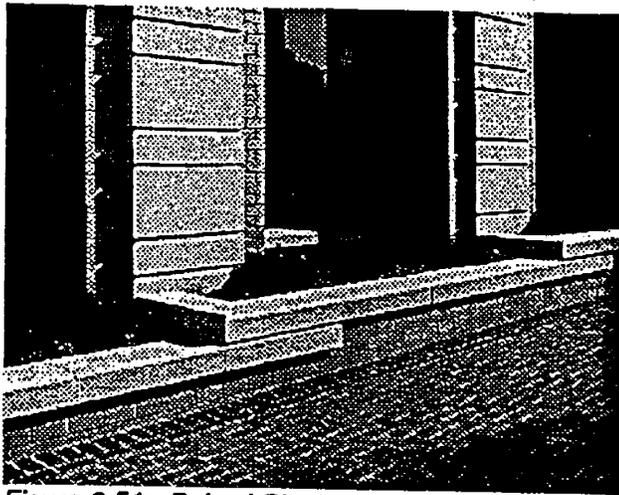


Figure 2-51: Raised Planter

Plantings may be installed in at-grade planting beds (flush with the sidewalk level) or in raised planters. Raised planters shall be constructed of masonry; they shall be designed as an integral part of the adjoining building in the same style, form, color and materials. Specific requirements for planting beds and raised planters may be found in each precinct chapter. Also see the information on seatwalls in Informal Seating in Section 2.5.1.

Required quantities of planting are expressed in terms of net plantable facade. Net plantable facade is that area along a building where plantings may be installed. Doorways, service and parking entrances and other areas requiring access are subtracted from the gross length of the facade to obtain the net plantable facade. Required planting in the building zone is then expressed in terms of a minimum percentage of this length and a minimum width.

Automatic mechanical irrigation systems shall be installed for these plantings. This system will be installed and maintained by the parcel owner.

Planting beds and planters shall be installed by the parcel owner. Maintenance of planting beds and planters including those in the curb zone shall be the responsibility of the parcel owner.

Additional requirements for specific plant types are below:

LAWN: All lawn areas shall be sodded when installed. Sod shall be "certified" or "approved" per the standards published by the Virginia Cooperative Extension Service. Grass cultivars shall be varieties recommended by the Virginia Cooperative Extension Service for the Alexandria area.

GROUNDCOVER: Groundcovers are used as a substitute for lawn in areas where foot traffic, size of area and location are not a factor to be considered. Suggested groundcovers are English Ivy, Pachysandra, Periwinkle, Liriope and Mondo Grass.

SEASONAL COLOR: Annuals, biennials and perennials may be used to provide seasonal flower color. Uses of seasonal color include highlighting entrances and signs. Seasonal color shall be restricted to the building zone and open spaces unless otherwise noted.

SHRUBS: Shrubs shall be planted in unified mass plantings. Individual shrubs spaced separately are to be avoided.

TREES: Trees other than those specified for street trees may be used in planting where there is adequate room. Parcel owners are encouraged to use flowering and ornamental trees especially for residential uses and entrance areas.

2.7.3 FREE STANDING PLANTERS

In certain precincts, the use of freestanding planters is encouraged. The design may be specified or left to the parcel owner to choose. When the design is left to choice, the planter color, material and type shall complement the adjacent architecture within the streetscape. Consideration should be given to the winter appearance of these planters, either by providing evergreen or other winter plantings or removing the planters to storage. Bare exposed soil in the winter will not be allowed.

Freestanding planters include pots, hanging baskets and window boxes and may be located in the building zone only unless otherwise noted. They shall not obstruct safe pedestrian movement. All

All shall be sufficiently large enough to provide an adequate amount of soil for growing plants in the hottest weather. Unless otherwise noted, the parcel owner is responsible for the maintenance of all freestanding planters. Specific criteria for each type follow:

POTS: Pots shall be twenty-four inch diameter minimum and hold at least two cubic feet of soil.

HANGING BASKETS: Hanging baskets are encouraged in certain precincts. They shall be used in the growing season only and stored in winter. Hanging baskets shall be twelve inches in diameter minimum. The hanging apparatus shall be clean, functional and inconspicuous.

WINDOW BOXES: Window boxes shall be at least eight inches wide and eight inches deep and appropriately scaled to the adjacent window.



Figure 2-52: Pots



2.8 SIGNS

Signs are an important and inevitable part of the streetscape, providing direction, information, and identity. In Carlyle, signs should be designed to contribute to the unity, interest, and character of the streetscape. To some degree, a minimalist approach is recommended, that is signs should be used only where needed to avoid clutter and confusion. Signs in Carlyle should be oriented to the specific user - either automobiles or pedestrians or both, as appropriate. All signs shall comply with Article IX of the Alexandria City Zoning Ordinance, unless otherwise indicated in these guidelines and approved by City Council as part of the Carlyle Coordinated Sign Program (SUP #97-0074).

2.8.1 REGULATORY SIGNS

All regulatory signs shall meet the requirements of the Virginia Department of Transportation and the City of Alexandria. These guidelines are intended to supplement those regulations.

Traffic Control and Restriction Signs:

Traffic control signs such as stop, yield, speed limit, driver information, and parking restriction signs shall be attached to the streetlights where possible, with a simple metal strap finished to match the streetlight pole.

When a separate post is required for these signs, the standard "U" type rolled rail steel post shall be used. The post shall be finished to match the street light pole. Posts shall be installed neatly in the sidewalk. The parcel owner shall provide posts as necessary.

Street Name Signs: Street name signs shall be attached to the nearest corner streetlight or stop light pole when possible. The sign plate will be the standard City of Alexandria design.

2.8.2 INFORMATION SIGNS

Information signs have more flexibility with regard to their design. The specific design will be left to the parcel owner subject to all applicable codes and the guidelines which follow.

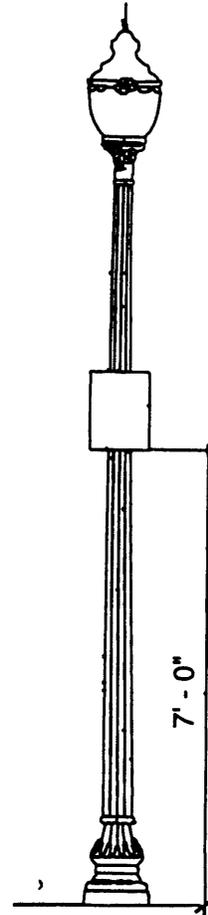


Figure 2-53: Sign Attached to Light Pole

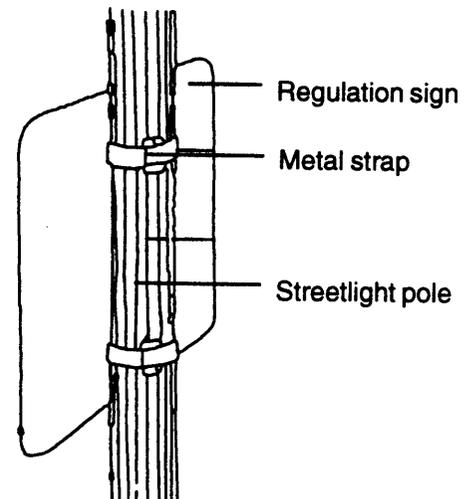


Figure 2-54: Sign Attachment

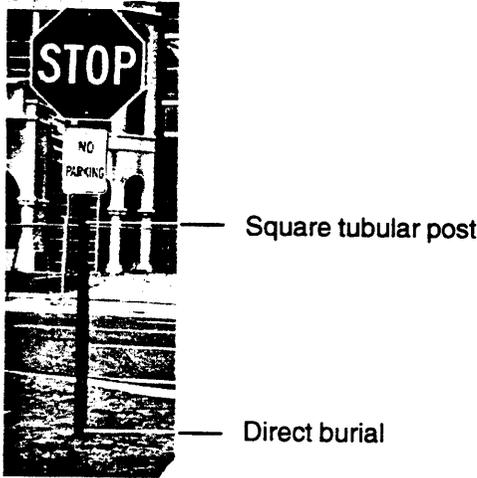


Figure 2-55: Sign on Steel Post in Walk

Signs shall be designed as an integral part of a building. They should relate in materials, color, and scale to the building architecture. Where illumination of the signs is allowed, the lighting shall be carefully integrated into the design of the sign, so as not to create glare or detract from the sign. Low intensity lighting is encouraged. Neon signs are permitted only with the approval of the Carlyle Design Review Board. Sign messages shall be limited to logos, names, and street address information. Signs and graphics should be simple and easily understood. Graphic symbols and word economy are encouraged. Slogans and advertisements are prohibited.

Business Names/Logo Signs: Business names and logos shall be limited to building mounted signs that are designed as an integral part of the architecture. Information on business name signs shall be limited to the name of the business and its logo. Wall mounted signs, transom signs, and sign bands are all acceptable for business signs. Business name signs that exceed 16 square feet in area require approval of the Carlyle Design Review Board.

Retail and Professional Signs: Retail and professional signs are typically associated with individual and multiple entrances. These signs are limited to entrances for a single use.

Retail and professional signs should be small in scale to address primarily the pedestrian. The sign should be of the same character as the architecture and well integrated with it. Each retail and professional business may have one sign, exclusive of off-site directory signs. Additional signs are permitted only with approval of the Carlyle Design Review Board. The following types of signs are permitted for retail and professional uses:

Wall mounted signs such as brass plaques are acceptable.

Box signs shall be located in sign bands above retail storefront windows. The location of the sign band shall be approved by the Carlyle Design Review Board at the time it reviews the overall building design. Sign bands should relate to the building's architecture and to neighboring buildings. Use of similar colors and lettering are encouraged. Box signs may be internally illuminated.



Figure 2-56: Business sign

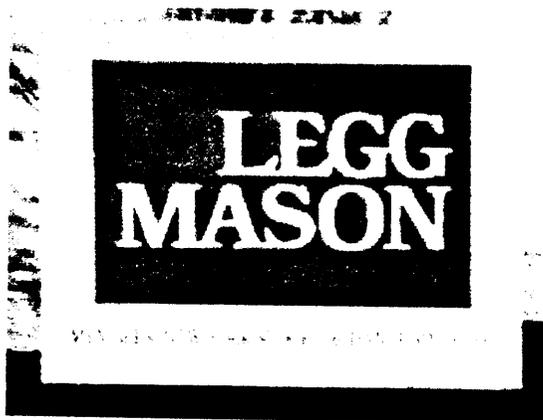


Figure 2-57: Professional sign



General Streetscape



Figure 2-58: Projecting Sign

Projecting signs over the sidewalk are encouraged in retail areas; these signs should be appropriately sized to human scale. The bottom of a projecting sign shall be at least ten feet above a sidewalk and at least 14 feet above a vehicular driveway. Projecting signs should be located near the primary entrance or doorway. Projecting signs should be aligned with other hanging signs. No more than one hanging sign should be installed for each business. Projecting signs may not be internally illuminated.

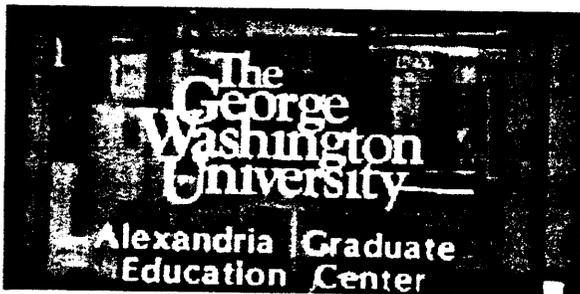


Figure 2-59: Storefront Sign

Signs applied to storefront windows are also acceptable. These signs should cover no more than twenty percent of the glass.

Signs on canopies or awnings giving the name of a retail establishment are also acceptable. Canopies and awnings should be constructed of a "sunbrella" type material (plastic coated canvas) Internally illuminated plastic awnings are not permitted.

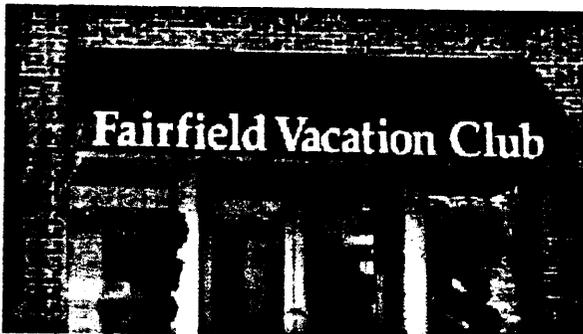


Figure 2-60: Sign on Awning

Building Identification Signs: Building identification signs consist of street address, building names and cornerstones. Building names and addresses shall be clearly visible from the street especially at night. They should be located in traditional locations such as over entrances, at the side of doors, or on porte cocheres. Building Identification Signs that exceed 16 square feet require approval of the Carlyle Design Review Board.

Freestanding signs will be allowed only with approval by the Carlyle Design Review Board. These freestanding signs shall be designed to reflect the scale, character, materials, and colors of the related building architecture. The sign height shall be no higher than five feet above finish grade.

The use of building cornerstones is encouraged especially in areas where pedestrians have visual access.

Accessory Information Signs: Accessory signs are signs which provide required information or directions. These are signs such as parking garage signs, signs identifying service entrances, et cetera.



Figure 2-61: Building Identification Sign

Accessory signs shall be permanently mounted to building walls. These signs shall be kept as



unobtrusive as possible. Messages shall be kept simple and straightforward. All accessory signs for a building will be coordinated in design for each building. They shall have similar sizes, proportions, and colors, and shall be made of the same materials.

Directories/Kiosks: Directories will be required in certain areas. These directories will be of a standard custom design, see Figure 2-64A, in order to be instantly identified as a directory. Directories shall contain a map to orient the user. Directories shall also be lit for night use. Directories will be maintained by the Carlyle Property Owners Association. In some places, the directories may be expanded to form kiosks which contain public telephones. Refer to Section 2.5.3 Public Services for more information on public telephones. See figure 2-64, amended, for Directory locations. A maximum of six directories is permitted. Directories to be located according to figure 2-64, amended, may be approved by staff, alternative locations require approval of the Carlyle Review Board.

Carlyle Monuments: Sign and other monuments for Carlyle may be installed by Carlyle Development Corporation. These monuments shall include brick and precast concrete materials in their design. The design will be tailored to the specific location and precinct. One example of monument signs is the Carlyle Lions located on either side of Dulaney Street. Other monument signs of differing design may be located at other prominent entrances with the approval of the Carlyle Design Review Board.



Figure 2-63: Accessory Information Sign



Figure 2-64A: Directory

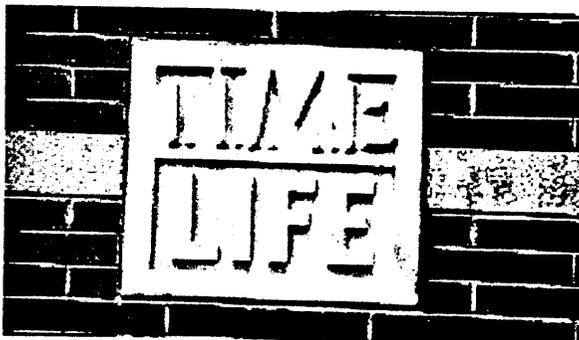


Figure 2-62: Cornerstone

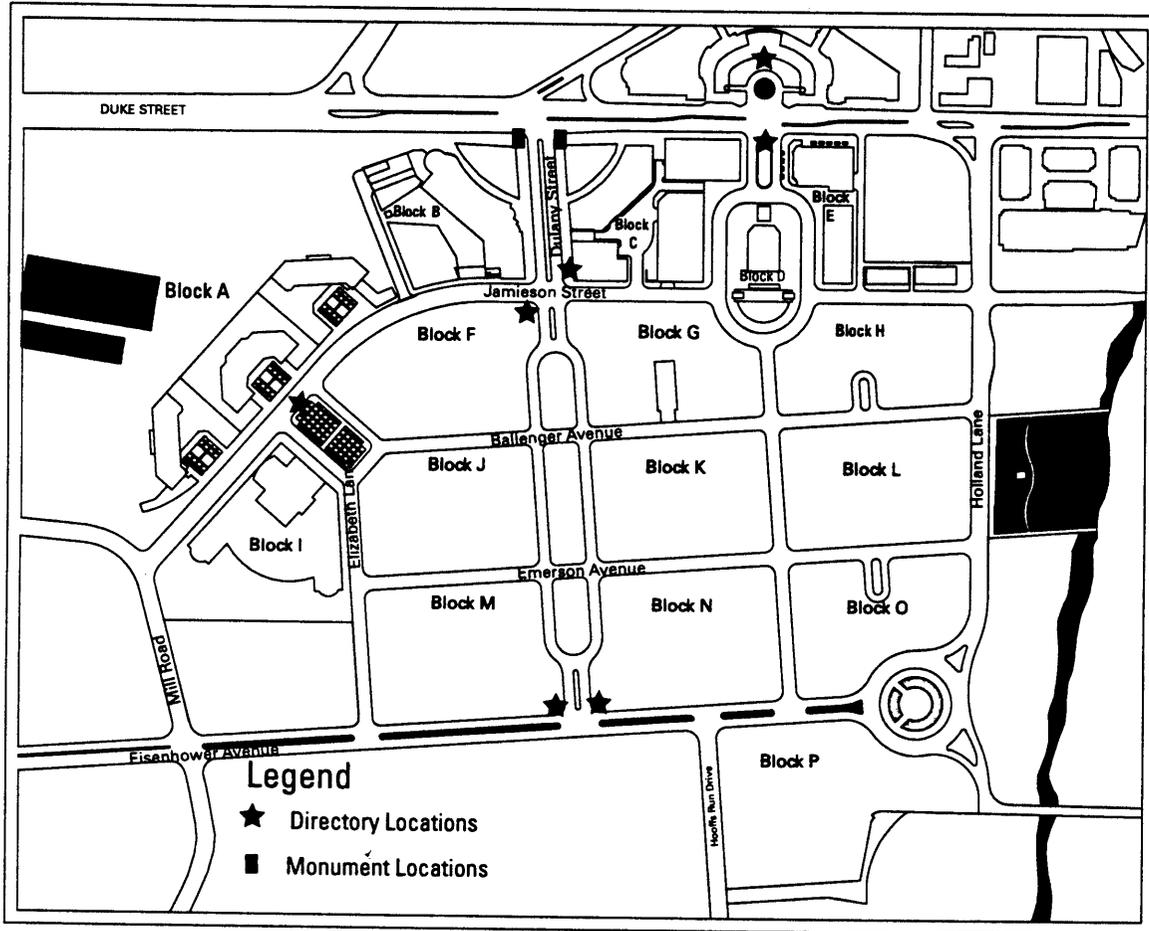


Figure 2-64: Directory Locations

Carlyle Real Estate Signs**3.30.98****CONSTRUCTION:**

1. Sign to be ground mounted
2. Sign may be either single or double-faced. Single-faced signs must be boxed in on back side. Sides and back of sign to be painted to match front face.
3. Sign panels are to be constructed of MDO plywood, shop-painted with exterior grade sign enamel.
4. Sign posts to be 4x4 pressure treated lumber. Exposed edge must be primed before painting to match face of sign.
5. All graphics to be applied, exterior-grade, 2 mil vinyl film.
6. Sign may not be illuminated.
7. Face panels to be screwed to supports post.
8. Sign may have any of the three predetermined Headers illustrated below. No other header shapes may be used. No sign without a header will be allowed.

COLOR AND FINISH:

1. Painted finishes to have semi-gloss finish.
2. Exposed screw heads to be painted out to match surrounding area.
3. Headers are to be painted as follows:
 - a) Stepped header to be Medium Blue to match PMS 308C
 - b) Peaked header to be Green to match PMS 561C
 - c) Rounded header to be Maroon to match PMS 208C
4. Main section of panel to be Dark Blue to match PMS 539C
5. Base to be painted Taupe to match PMS 451C.
6. Copy on Main Section must be white.
7. Copy on Header to be:
 - a) Lion: Line art prints Dark Blue to match PMS 539C. Inside to print Gold to match PMS 131C.
 - b) Type and Swash: Line art prints white. Inside to drop out to expose background color.
8. Vinyl equivalents for Lion colors are:
 - a) Gold PMS 131C - 3M Harvest Golds #105 and Calon II "Imitation Gold" #59
 - b) Dark Blue PMS 539C - 3M Light Navy #197 and Calon II "Midnight Blue" #65.

TYPE STYLE:

1. All copy to be Galliard. (Galliard, Galliard Bold, Galliard Italic and Galliard Bold Italic are acceptable.)
2. Artwork for Carlyle Lion and Logo is available from Carlyle Development Corporation.
3. Logos of Leasing Agents are allowable.
4. If double-faced, information on rear face must match front face.
5. Sign layout must be submitted to Carlyle Development Corporation for approval.
6. 3M Scotchprint "photos" may be used to illustrate project.

LOCATION:

1. Location of sign must be submitted to Carlyle Development Corporation for approval.
2. Posts must set down into earth at a minimum of 3'-0".

Figure 2-66: Real Estate Sign Guidelines



General Streetscape

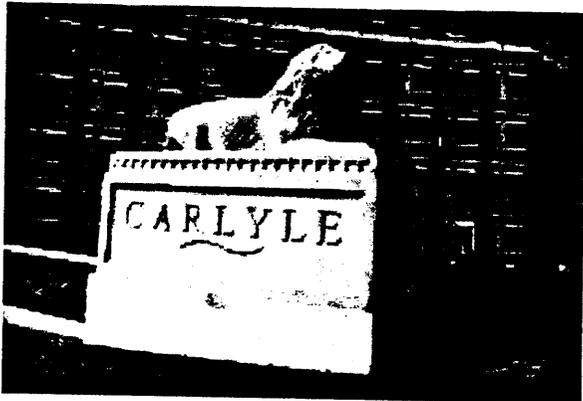


Figure 2-65: Carlyle Monument at Dulany Street

2.8.3

Real Estate Signs

Signs used for the purpose of advertising the sale, lease or other use of real estate shall be designed in accordance with the graphic standards for real estate signs specified in the Carlyle Coordinated Sign Program, as may be amended from time to time. The specific design will be left to the parcel owner subject to program guidelines for use of color, letterstyle, materials and finishes (Figure 2-66). Carlyle Development Corporation must approve the proposed design prior to installation.

Such signs are to be free standing and may be double-faced, not to exceed 50 square feet per face. No sign type prohibited by the Alexandria City Zoning Ordinance may be used for real estate advertising purposes. Messages shall be kept simple and straightforward and may include graphics such as building renderings, site plans, and building or business logo as well as information on leasing contacts, project description, project credits (developer, architect, financial institution, etc.).

Real estate signs may be placed in any designated location shown in Figure 2-67, with the prior approval of Carlyle Development Corporation. Alternative locations require the approval of Carlyle Design Review Board. In general, one real estate sign is permitted per lot for a given project. Carlyle Development Corporation may approve one additional real estate sign for a given project, up to a maximum of two per project.

Contractor or Subcontractor Signs: Individual signs advertising contractors, construction companies, financial institutions, architectural firms, engineering firms, etc. shall be limited to not more than three per lot, including one general contractor sign of not more than 24 square feet and two subcontractor signs of not more than 8 square feet each. Such signs may be freestanding or attached to construction equipment or construction trailers. Carlyle Development Corporation must approve placement of any freestanding sign prior to installation. Sign(s) exceeding the allowable number will be removed at owner's expense. Information on contractor or subcontractor signs will be limited to company names and/or logos.

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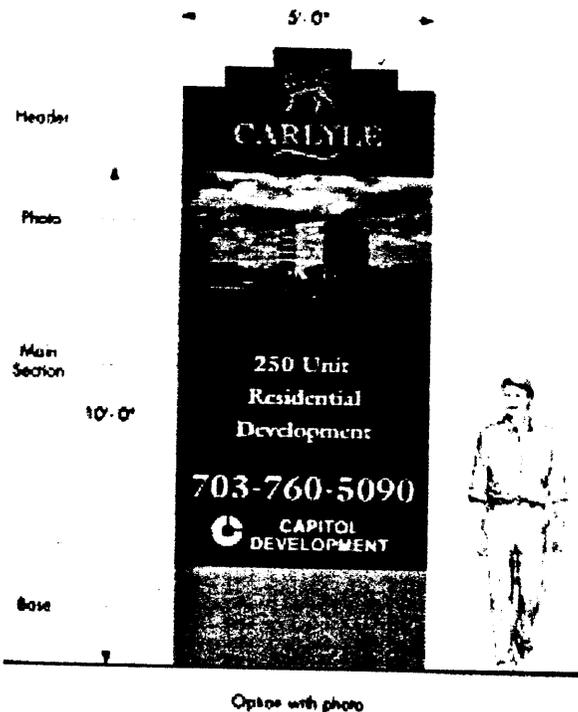


Figure 2-66A: Carlyle Real Estate Sign



General Streetscape

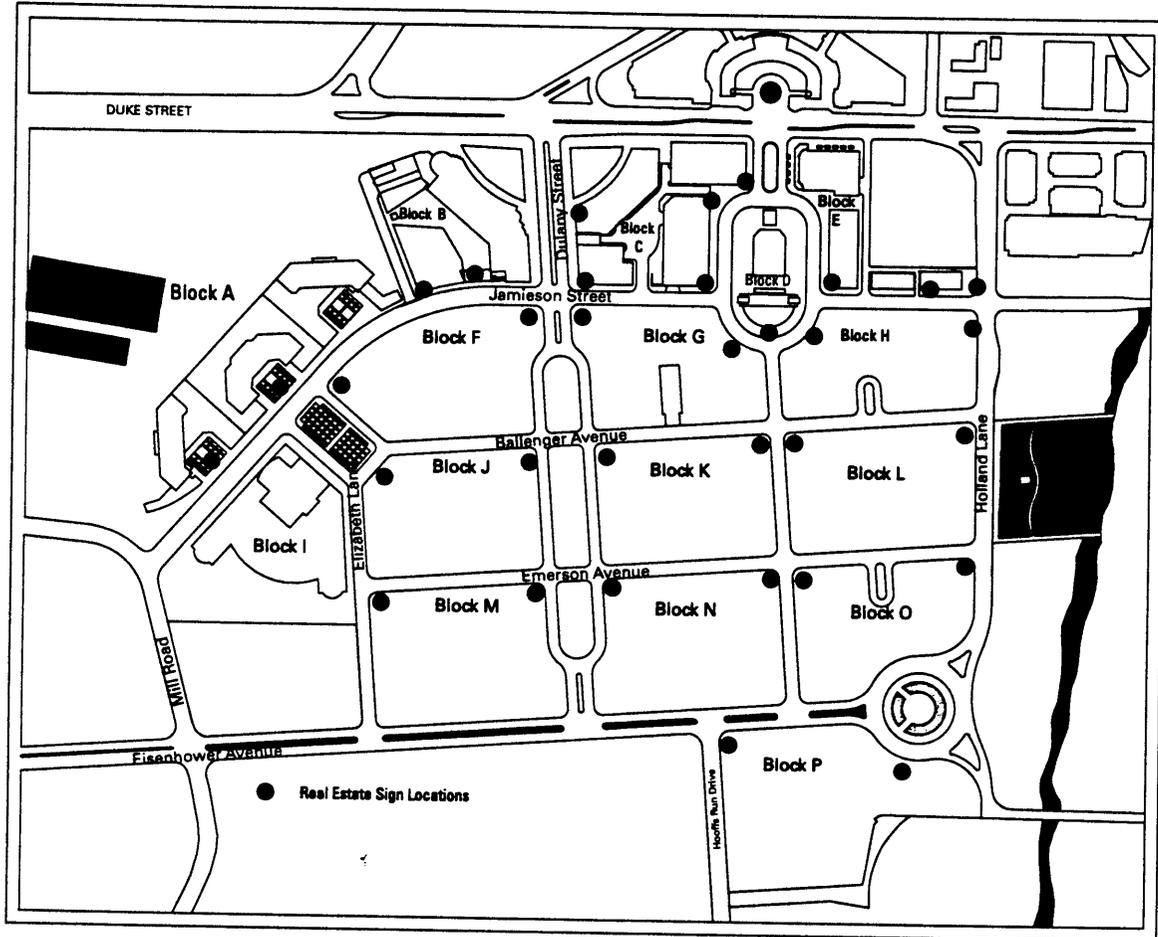


Figure 2-67: Real Estate Sign Locations

SUP 98-0023

5'-0"



CARLYLE

1, 2 and 4 Story
Office Buildings
Offering a Total of
270,000 sq.ft.

Delivers 1999

703-760-5090



**CAPITOL
DEVELOPMENT**

10'-0"



CARLYLE

250 Unit
Residential
Development

Delivers 2000

703-760-5090



**CAPITOL
DEVELOPMENT**



CARLYLE

Mixed Use
255,000 sf Office/
20,000 sf Retail
Development

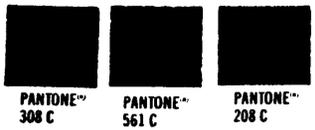
Delivers 2001

703-760-5090



**CAPITOL
DEVELOPMENT**

24



2.9 UTILITIES

In Carlyle utilities will be underground as required by the Special Use Permit, condition #R-7. Normal practice is for utilities to be installed first. Care must be exercised to coordinate utility locations so that the streetscape design is not harmed. Specific requirements for utilities follow.

2.9.1 UTILITY LINES

All utility lines shall be placed underground as required by the Special Use Permit Condition #R-7. Utility lines shall be routed to minimize disruption of street tree planting troughs. Utility lines shall cross tree planting troughs as perpendicular as possible to the trough and curb.

2.9.2 FIRE HYDRANTS

Fire hydrants will be installed as needed according to the City standards. Fire hydrants shall not be located in tree wells or planter areas; they shall be located in pavement areas only.

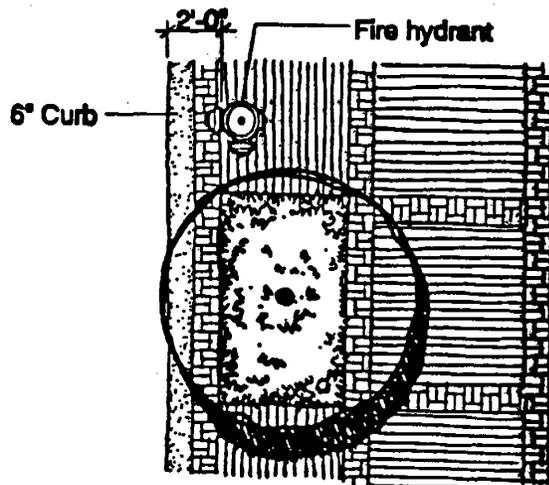


Figure 2-66: Utility Locations

2.9.3 MANHOLES

No manholes shall be permitted within the sidewalks; all manholes shall be located in the street.

2.9.4 UTILITY VAULTS

Utility vaults such as electrical transformers are prohibited within the sidewalk by Condition R-7 of the Special Use Permit. Utility vaults shall be located within the buildings, within landscape areas behind the build-to line or in landscape areas within the landscape easements. Utility vaults shall be adequately screened with walls or landscape plants.

2.9.5 SIDEWALK DRAINAGE

Sidewalk drainage flumes and checker plates are prohibited. Rain spouts or other drains shall be connected directly to the storm drain system. Water shall not be drained in a concentrated manner across sidewalks.



2.10 ARCHITECTURAL PROJECTIONS

Details in architecture provide variety and interest, and may add substantially to the character of the streetscape. Details which may project into the streetscape include awnings, canopies, garden courts, marquees, porte cocheres, stairs and stoops. The color, sizes and proportions of these projections shall relate to the building architecture and to the streetscape context. Specific requirements may be given on a precinct basis. Please also refer to Section 2.3.2 Building Entrances.



Figure 2-67: Canopy

2.11 TRANSITIONAL AREAS

There are several transitional areas within the streetscapes of Carlyle. These are transitions within the same block, transitions between precincts and transitions to areas adjacent to Carlyle.

2.11.1 TRANSITIONS WITHIN THE SAME BLOCK

Because of the urban nature of Carlyle and the mixed uses, there may be multiple buildings on one block with differing characters. One of the purposes of these guidelines is to provide a consistent streetscape design to unify the development. The streetscape at transitions between developments in a block shall be constructed to be continuous and appear seamless. Pavement patterns, tree spacing, streetlight spacing, et cetera shall match. When parcels adjoin an undeveloped parcel, the streetscape shall be finished in a manner that will allow for ease of continuing the construction of the adjoining segment.

2.11.2 TRANSITIONS BETWEEN PRECINCTS

Transitions between precincts normally occur at street intersections. For each transition, there is a dominant precinct. The streetscape design of the dominant precinct is the one which will determine the intersection design. The goal is to create a

smooth transition from the dominant precinct to the subordinate precinct. One to several of the dominant precinct's streetscape elements will turn the corner to varying extents. Only in rare situations will all elements change abruptly at the same line. The intention is to feather the changes from one precinct to the next.

2.11.3 TRANSITION TO AREAS ADJACENT TO CARLYLE

The streetscape shall extend to the boundaries of Carlyle with the appropriate precinct design. The precinct design will be clearly stated to announce and identify Carlyle as a place. The design will need to be adapted as necessary to make the physical connections to existing streets and sidewalks. When adaptations are made for such connections, they should respect the design intent and character established by the streetscape design.

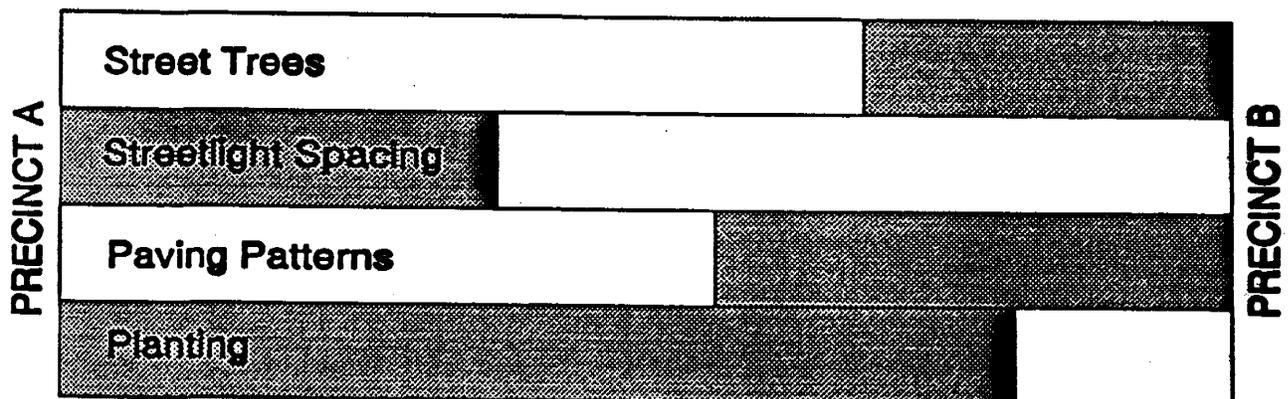


Figure 2-68: Precinct Transition



2.12 SPECIAL AREAS

In addition to the major open spaces, the precincts may have other areas with special design requirements. These areas will be addressed in each precinct section in terms of the precinct design.

APPENDIX I

Reference Matrix of Required Elements

This matrix will provide the uses of these guidelines a quick reference to determine requirements and options for each streetscape precinct in Carlyle. The matrix is divided into the three streetscape zones to provide a general location for the elements. Additional specific requirements for location as well as quantity are given in the appropriate sections of the guidelines.

CURB ZONE ● = preferred alternative fulfilling requirement ○ = alternative - fulfills requirement ◇ = optional	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
BUS STOPS (2.3.3)		●		●			Subject to bus routes
CROSSWALKS (2.3.4)							
Standard	●	●	●	●	●	●	
PAVING (2.4.1)							
King William Range Brick		●	●	●	●	●	
Old Virginia Brick #24	●						To match King Street Station
Special Paving			◇				Courthouse Square only
PAVING PATTERNS (2.4.1)							
Grid with Diagonal Field	●						Running bond to match King Street Station
Running Bond			●	●	●		
Diagonal Basket Weave		●					
Perp. Running Bond			●				
Herringbone	●					●	Along Duke Street only at Carlyle Precinct
CURBS (2.4.2)							
6" concrete curb	●	●	●	●	●	●	



CURB ZONE	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
● = preferred alternative fulfilling requirement ○ = alternative - fulfills requirement ◇ = optional							
SEATING (2.5.1)							
Benches	●		●				
STREET FIXTURES (2.5.2)							
Trash Receptacles	●	●	●	●	●	●	
Flags	●	●	●				At street corners
Bike Racks	◇	◇	◇	◇	◇	◇	
Bollards	◇	◇	◇	◇	◇	◇	
PUBLIC SERVICES (2.5.3)							
U.S. Mailboxes	◇		◇	◇		◇	Face pedestrian way, no curb drops
Newspaper Machines	◇	◇	◇	◇			
STREETLIGHTS (2.6.1)							
Double Luminaire	●	●	●				At strategic locations
Single Luminaire		●	●	●	●	●	
Gadsby	●	●					On Duke Street only
STREET TREES (2.7.1)							
Double Row				●	●		Eisenhower Avenue and Rotary
Single Row	●	●	●	●	●	●	West side of Holland Lane only
Curbed Tree Wells		●					
Tree Grates	●		●				Carlisle Square & Jamieson Square only
At Grade Tree Wells	●		●			●	
In Lawn				●	●		

CURB ZONE ● = preferred alternative fulfilling requirement ○ = alternative - fulfills requirement ◇ = optional	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
FREESTANDING PLANTERS (2.7.3)							
Pots	●						



PEDESTRIAN ZONE	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
● = preferred alternative fulfilling requirement ○ = alternative - fulfills requirement ◇ = optional							
PAVING (2.4.1)							
King William Range Brick		●	●	●	●	●	
Old Virginia Brick #24	●						To match King Street Station
Special Paving		◇	◇	◇		◇	Major lobby entrances
PAVING PATTERNS (2.4.1)							
Grid with Diagonal Field	●						Running bond to match King Street Metro
Running Bond				●	●		
Diagonal Basket Weave		●					
Perp. Running Bond			●				
Herringbone						●	
STREET FIXTURES (2.5.2)							
Clocks, Art and Special Features	◇	◇	◇	◇	◇	◇	
Bollards	◇	◇	◇	◇	◇	◇	Service & parking entrances; drop offs & lay-bys
INFORMATION SIGNS (2.8.2)							
Directories	●	●	●	●			

BUILDING ZONE	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
<p>● = preferred alternative fulfilling requirement</p> <p>○ = alternative - fulfills requirement</p> <p>◇ = optional</p>							
PAVING (2.4.1)							
King William Range Brick		●	●	●	●	●	
Old Virginia Brick #24	●						To match King Street Station
Special Paving	◇	◇	◇	◇		◇	Major entrances
PAVING PATTERNS (2.4.1)							
Grid with Diagonal Field	●						Running bond to match King Street Station
Running Bond				●	●		
Diagonal Basket Weave		●					
Perp. Running Bond			●				
Herringbone						●	
SEATING (2.5.1)							
Benches-Public Street	◇		◇	◇			
Seatwalls		●	○			◇	
Tables & chairs	●	◇	◇				
STREET FIXTURES (2.5.2)							
Flags	◇	◇	◇	◇	◇	◇	
Banners	◇		◇				
Clocks, Art, Etc.	◇	◇	◇	◇	◇	◇	
Fences, Gates, Columns and Walls	◇	◇	◇		○	◇	Gardens Courts
Bollards	◇	◇	◇	◇	◇	◇	Parking/service entrances design element



BUILDING ZONE	CARLYLE PRECINCT	DULANY PRECINCT	JAMIESON PRECINCT	EISENHOWER PRECINCT	HOLLAND PRECINCT	BALLENGER PRECINCT	REMARKS
<p>● = preferred alternative fulfilling requirement</p> <p>○ = alternative - fulfills requirement</p> <p>◇ = optional</p>							
PUBLIC SERVICES (2.5.3)							
Newspaper Machines	◇	◇	◇	◇			
Public Telephones	◇		◇	◇			Well lit, visible, secure area, out going calls only, free standing, or in directories
PLANTING BEDS AND PLANTING (2.7.2)							
Seatwall Height Planter		●	○			◇	Only allowed at lobby entrances within Ballenger Precinct
Curbed		○	●			○	
At Grade	○			●	●	○	
Pots	●					○	Ballenger Precinct individual additions
Window Boxes	○					○	
ARCHITECTURAL PROJECTIONS (2.10)							
Awnings	●						
Canopies		◇					



APPENDIX II

Reference List of Specified Products

This reference list will give the user of these guidelines the name and address of the manufacturer or distributor of the products specified within these guidelines. The specifications are not intended to be proprietary specifications; substitutions of equal quality and design may be made with the approval of Carlyle Development Corporation and other parties where required.

For the specific quantity and location of these items, see the appropriate section(s) of the guidelines.

ITEM	SPECIFICATION	SOURCE	REFERENCE
Benches	Restoration Series #2118, 2119 and 2123	TimberForm Columbia Cascade Company Distributed by: Mid-Atlantic Products 3 Church Circle, Suite 252 Annapolis, MD 21401 (301) 858-7331	2.5.1
Bicycle/Moped Rack	Bicycle Sentry, Cat No. 900	Canterbury International, Inc. P.O. Box 5730 Sherman Oaks, CA 91413 (213) 936-7111	2.5.2
Bollard, decorative	Princeton Cast Iron Embedded (direct burial) Bollard	Spring City Electrical Mfg. Co. P.O. Drawer A Spring City, PA 19475 (215) 948-4000	2.5.2
Brick Pavers	King William Range Brick Pavers (4" x 8")	United Materials Manassas, VA 22110 (703) 631-1105	2.4.1
	Old Virginia #24 Brick Pavers (4" x 8")	United Materials Manassas, VA 22110 (703) 631-1105	2.4.1
Bus Shelter	Prefabricated shelter system with barrel vault roof and decorative grillwork	Columbia Equipment Company 180-10 93rd Avenue Jamaica, NY 11433-1499 (718) 658-5900	2.3.3



ITEM	SPECIFICATION	SOURCE	REFERENCE
Precast Concrete Curbing for Dulany Precinct	#1006 with light sandblast	Maryland Cast Stone, Inc. 14820 Southlawn Lane Rockville, MD 20850	4.41
Pots	Lorraine Series Bowls Florence Series Pots	Magnalite Systems, Inc. 2900 Lockheed Way Carson City, NV 89706 (800) 356-2462	2.7.2; 3.7.2
Public Telephone Enclosure	Intermediate Enclosure #CK20842 L35A/61	Clark Specialty Company, Inc Route 54 Hammondsport, NY 14840 (607) 569-2191	2.5.3
Streetlights	City of Alexandria "Gadsby": Wayzata Series Luminaire	Sterner Lighting Systems Winstead, MN 55395	2.6.1; 3.6.1
	Franklin Lamp Post	Spring City Electric Mfg. Co. P.O. Drawer A Spring City, PA 19475 (215) 948-4000	2.6.1; 3.6.1
	Edison III Luminaire (fitter and ballast assembly)	General Electric Company Distributed by: Commercial Lighting Sales, Inc. 6797 Dorsey Road, Suite 3 Elkridge, Maryland 21227 (301) 621-1650	2.6.1
	Victorian III Series Refractive Globe with Decorative Band and Finial	Hadco Outdoor Lighting Distributed by: Genlyte 7120 Columbia Gateway Drive, Suite 100 Columbia, Maryland 21046	2.6.1



ITEM	SPECIFICATION	SOURCE	REFERENCE
Streetlights (cont.)	Washington Series 405 fluted tapered fiberglass composite fourteen foot pole; for double streetlights, Celtic Series thirty-six inch double arm	W.J. Whatley, Inc. Distributed by: Ambiance Lighting P.O. Box 1484 1532-F Pointer Ridge Professional Center Mitchellville, MD 20717	2.6.1
Trash Receptacles	Iron Site Bethesda Series #S-42 with spun steel dome #S-2	Victor Stanley, Inc. Brick House Road Dunkirk, MD 20754 (800) 368-2573	2.5.2
Tree Grates	Centennial Series 5' x 8'	Ironsmith, Inc. Distributed by: Mid-Atlantic Products 3 Church Circle, Suite 252 Annapolis, MD (301) 858-7331	5.7.1
	O.T. Series, 5' square	Urban Accessories First Street and Avenue A 20004 144th Avenue, NE Woodinville, WA 98072 (206) 487-0488	3.7.1



3 CARLYLE PRECINCT

Additional information and special requirements for the streetscape for Carlyle Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

<p>3.1 Design Concept and Character 3-3</p> <p>3.1.1 Context 3-3</p> <p>3.1.2 Use 3-4</p> <p>3.1.3 Imagery 3-4</p> <p>3.2 Major Open Space 3-5</p> <p>3.3 Streetscape Design 3-7</p> <p>3.3.1 Typical Layout and Dimensions 3-7</p> <p>3.3.2 Building Entrances</p> <ul style="list-style-type: none"> •Major Lobby Entrance •Multiple Entrance <p>3.3.3 Vehicular Access Zones ... 3-8</p> <ul style="list-style-type: none"> •Drop Offs/Lay-Bys <p>3.4 Hardscape 3-9</p> <p>3.4.1 Paving 3-9</p> <ul style="list-style-type: none"> •Standard Sidewalk Paving •Pavement Edges •Special Paving •Planter Bed Edges <p>3.5 Street Furniture 3-11</p> <p>3.5.1 Seating 3-11</p> <ul style="list-style-type: none"> •Formal Seating: Benches <p>3.5.2 Street Fixtures 3-11</p> <ul style="list-style-type: none"> •Trash Receptacles •Fences, Gates, Columns and Walls <p>3.6 Lighting 3-12</p> <p>3.6.1 Streetlights 3-12</p> <p>3.7 Planting 3-13</p> <p>3.7.1 Street Trees 3-13</p> <p>3.7.2 Planting Beds and Planters 3-13</p> <p>3.7.3 Free Standing Planters ... 3-13</p>	<p>3.8 Signs 3-15</p> <p>3.8.2 Information Signs 3-15</p> <ul style="list-style-type: none"> •Retail and Professional Signs •Directories/Kiosks •Carlyle Monuments <p>3.10 Architectural Projections 3-16</p> <p>3.10.1 Awnings/Canopies 3-16</p> <p>3.10.2 Porte Cocheres 3-16</p> <p>3.10.3 Steps and Stoops 3-16</p> <p>3.11 Transitional Areas 3-17</p> <p>3.11.2 Transitions between Precincts 3-17</p> <ul style="list-style-type: none"> •Ballenger Precinct Transition •Dulany Precinct Transition •Jamieson Precinct Transition <p>3.11.3 Transitions to Areas Adjacent to Carlyle 3-19</p>
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3 CARLYLE PRECINCT

Carlyle Precinct is the most lively public place in Carlyle. It is the heart, a marketplace with a traditional character. It includes the existing King Street Station and should be integrated with it.

Carlyle Precinct is located on the north side of Carlyle extending across Duke Street to King Street Station. Jamieson, Ballenger and Dulany Precincts surround it on the other sides.

3.1 DESIGN CONCEPT AND CHARACTER

Carlyle Precinct exemplifies the first design principle in the design report to maximize active ground floor uses. It will be like an old town square with shops, office and residences focused on an open space. The Design Report and Development Plan describe it as a:

...most critical area in terms of scale and characteristics as it integrates with the King Street Station Project and older buildings along Duke and King Streets. The precedents for this square include Palmer Square in Princeton, New Jersey and the Square at City Hall in Alexandria. The space will be framed by six-to-seven story (77' high) office and residential buildings. Four-story pavilion buildings of a more traditional scale and character will announce this special place at Duke Street. Ground floor retail uses will line the plaza. It will be a focus for public activity both during and after regular office hours, and the setting for festivals, markets, fairs and other large gatherings of people.

3.1.1 CONTEXT

SITE PLAN: Carlyle Precinct focuses on the Carlyle Square with the four story retail building at the south end. The square is surrounded by buildings, creating a sense of enclosure around the square. At Duke Street, two pavilion buildings help to forge a strong relationship to King Street Station, by framing and focusing the John Carlyle Street axis, onto the

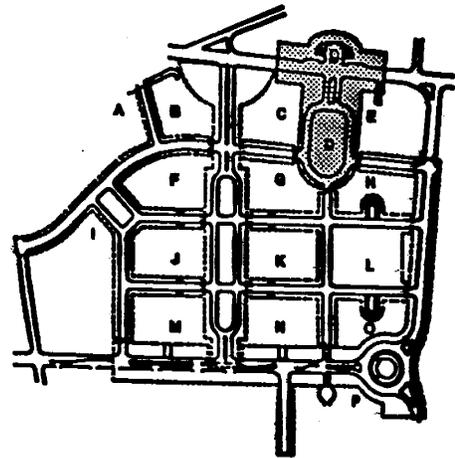


Figure 3-1: Carlyle Precinct

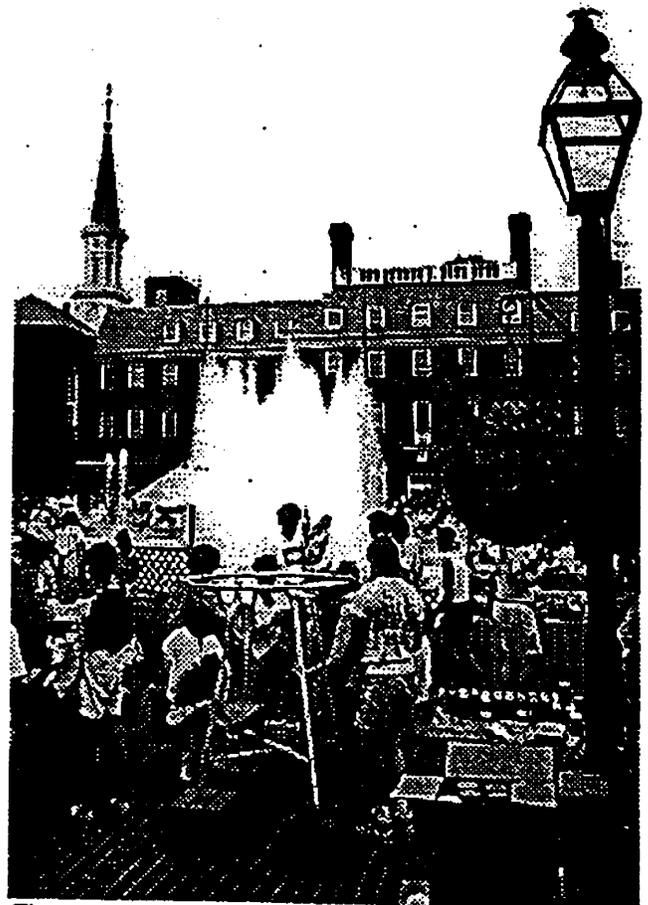


Figure 3-2: Market Square, Alexandria, Virginia



King Street Station arcade across Duke Street. Multiple retail entrances facing the square add liveliness, color and detail to the pedestrian sidewalk.

CIRCULATION: Vehicular traffic will be one way around the square and will be mainly local traffic. Parallel parking along the street will slow the movement of cars on the street. Pedestrian movement will be free flowing especially across the square; the streetscape design should promote this free flowing pedestrian movement.

OPEN SPACE: The open space, Carlyle Square, is defined by the retail buildings on Block D and the building walls of Blocks C and E. At the north end of the square, a small pavilion will house access to parking beneath the square, and possibly a cafe. This would be a place where people would come and go.

3.1.2 USE

The predominant use in Carlyle Precinct is mixed office/residential/retail. The primary first floor use is retail. The interaction between pedestrians and the shop windows is a strong indoor/outdoor relationship. Merchants want to invite window shopping and to maintain high visibility along the street. The shops, restaurants and cafes are public uses. The residential entrances in Carlyle Precinct by contrast are semi-public. The mix of public and semi public retail, office and residential uses will make Carlyle Precinct a hub of activity through much of the day into the evening as people come and go to work, shop, meet friends and come home.

3.1.3 IMAGERY

Carlyle Precinct is the heart of Carlyle and will be a vibrant, colorful marketplace with its retail shops surrounding the largest square. Markets and fairs, shops, restaurants and outdoor cafes, and evening nightlife will all contribute to this atmosphere. The sidewalk is the center of activity in Carlyle Precinct. Carlyle Precinct will be very much like Market Square in Old Town, Alexandria.



Figure 3-3: Old Town, Alexandria, Virginia

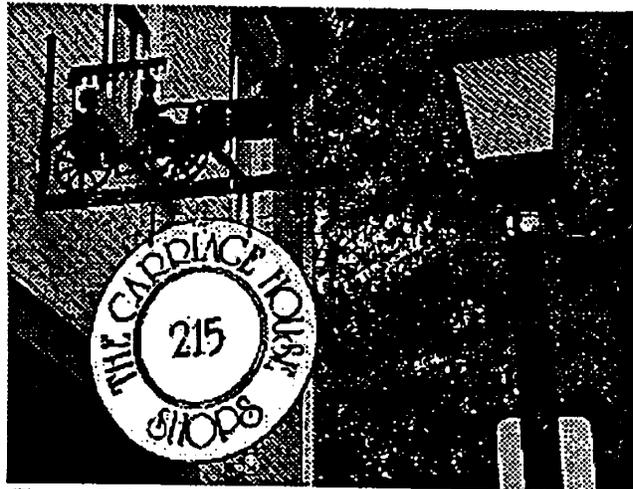


Figure 3-4: Detail - Old Town, Alexandria



3.2 MAJOR OPEN SPACE

The major open space of Carlyle Precinct is Carlyle Square. Carlyle Square is just over two acres in size. The square shall be a pedestrian oriented open space that is flexible for many uses such as formal and informal gatherings including public performances and lunches. The square should have transparent edges and allow for free pedestrian access across the space along the streets and to the shops. The square shall have a central open space which focuses attention onto a four story retail building. (See Figure 3-5: Carlyle Square Open Space Concept) No physical barriers should be placed to obstruct the view into or across the square; steps and other vertical barriers shall be kept low.

Carlyle Square will be an integral part of the precinct by the repetition and placement of street furnishings. The square's street edge shall be designed to be identical to the street edge along the buildings across John Carlyle Street East and West. The use of a grid paving pattern within the space is encouraged. Fixed and moveable seating shall be provided within the square.

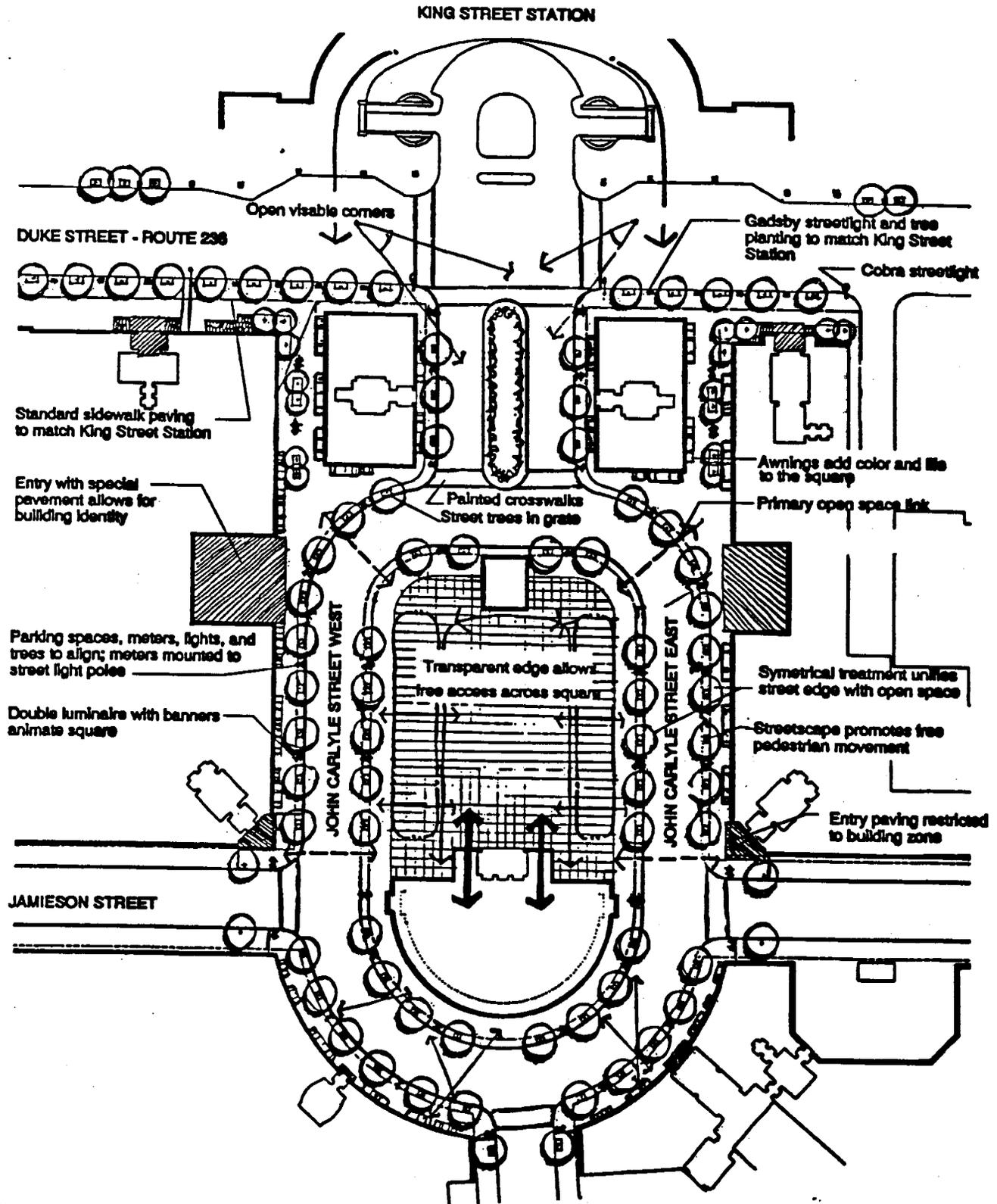


Figure 3-5: Carlyle Square Open Space Concept

3.3 STREETScape DESIGN

3.3.1 TYPICAL LAYOUT AND DIMENSIONS

In Carlyle Precinct the three streetscape zones will be the least distinct. The curb zone shall be six and one half feet wide; it shall consist of a six inch curb with a one foot band and the first five foot square pavement module. The pedestrian zone is immediately behind the curb zone and shall be a minimum of ten feet wide; the building zone shall be a maximum of three feet wide.

The sidewalk in Carlyle Precinct is based on a five foot square module similar to the paving at King

Street Station. This module is set into a grid of four inch banding. The grid shall be aligned to the north-south axis of Carlyle Square, except the streetscape along Blocks G and H where the grid shall be radial to the street.

3.3.2 BUILDING ENTRANCES

MAJOR LOBBY ENTRANCE: In order to maintain a continuous pedestrian zone along the street, any special design features such as special paving shall not extend beyond the building zone.

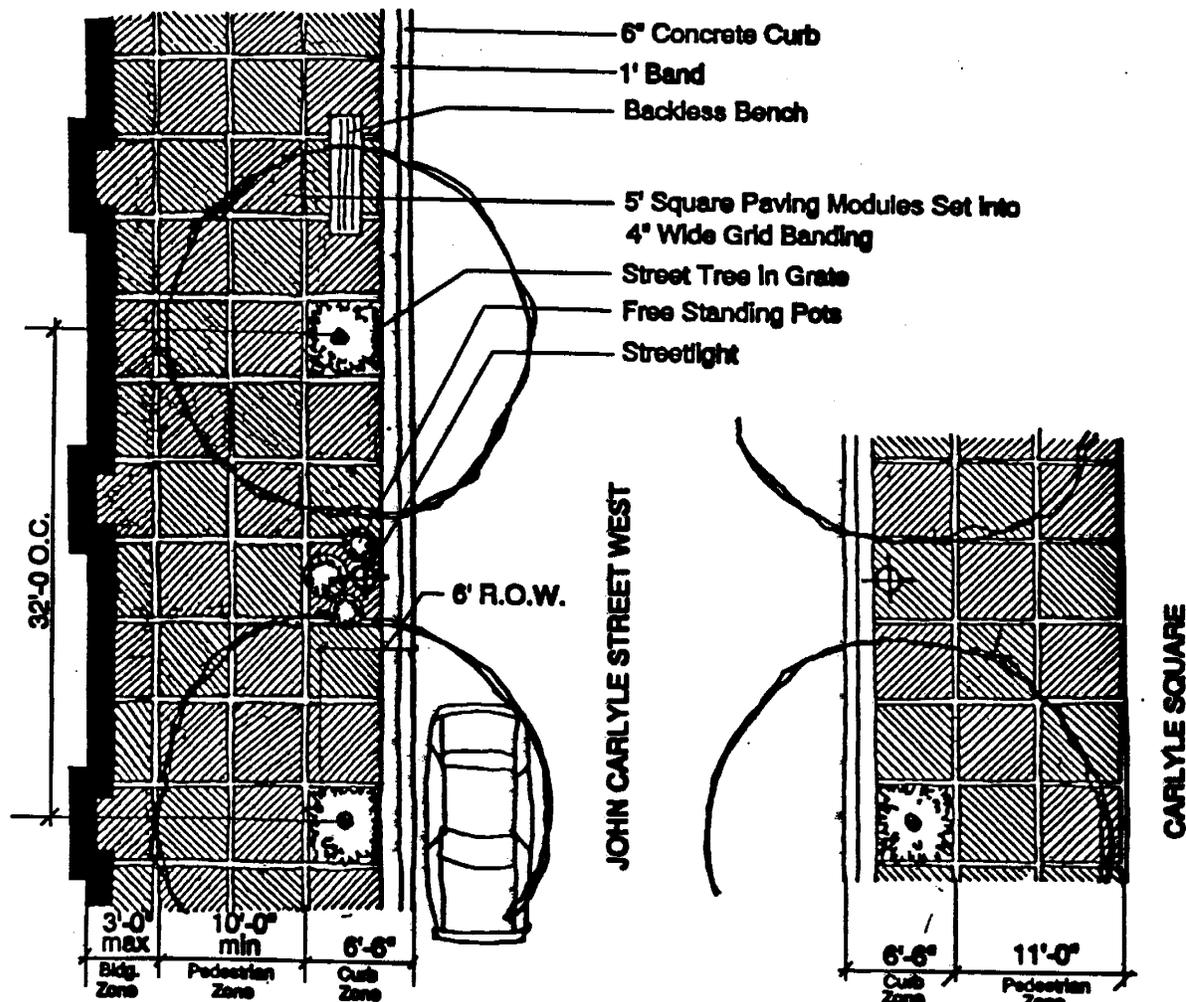


Figure 3-6: Carlyle Precinct Typical Streetscape



MULTIPLE ENTRANCE: Individual expression of retail establishments through planting, lighting, awnings banners and signs is strongly encouraged.

3.3.3 VEHICULAR ACCESS ZONES

DROPOFFS/LAYBYS: Dropoffs and laybys are not permitted in Carlyle Precinct.



3.4 HARDSCAPE

3.4.1 PAVING

STANDARD SIDEWALK PAVING: There are two standard sidewalk pavings for Carlyle Precinct. The primary paving shall be the Old Virginia #24 brick paver laid in an alternating five foot square modules of forty-five degree diagonal running bond pattern to match King Street Station. The five foot square modules shall be laid in a grid of a single brick stretcher band. This grid will be aligned to the centerline of John Carlyle Street except for along blocks G and H where the grid shall be radial with the street. Tree grates for street trees shall be coordinated with the grid. A one foot band shall be provided at the curb in a running and stack bond oriented to take the place of the single brick band. The diagonal grid paving shall be used on all sidewalks around the square and extend to Duke Street. It shall extend west along Duke Street sixty feet from the John Carlyle Street West build-to-line, and east along Duke Street to the edge of Carlyle to encompass all retail storefronts.

For the remainder of Duke Street in Carlyle Precinct, a second standard sidewalk paving shall be used. This paving shall be the Old Virginia #24 paving brick laid in a herringbone pattern to match the paving at King Street Station across Duke Street. Between the two sidewalk pavings a transitional paving similar to the existing transitions at King Street Station shall be used. This transition consists

of a panel of brick pavers laid in a running bond pattern parallel to the street. This panel shall be a minimum of 5'-0" in length and extend to the full width of pavement areas of sidewalk. Where possible it shall be aligned to an architectural feature. A one foot running and stack bond band shall be provided along the curb at Duke Street.

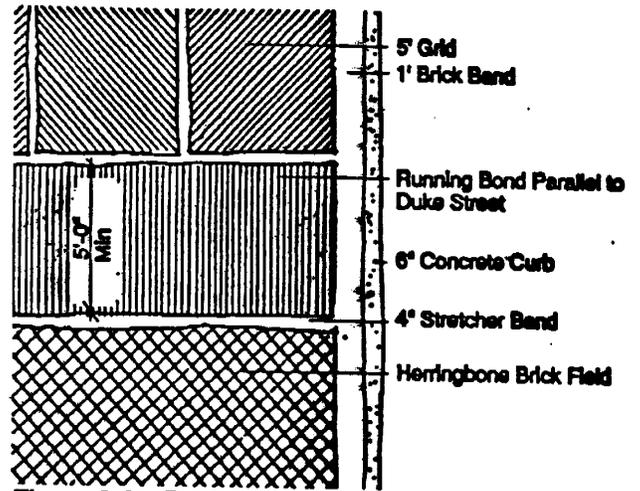


Figure 3-8: Paving Transition

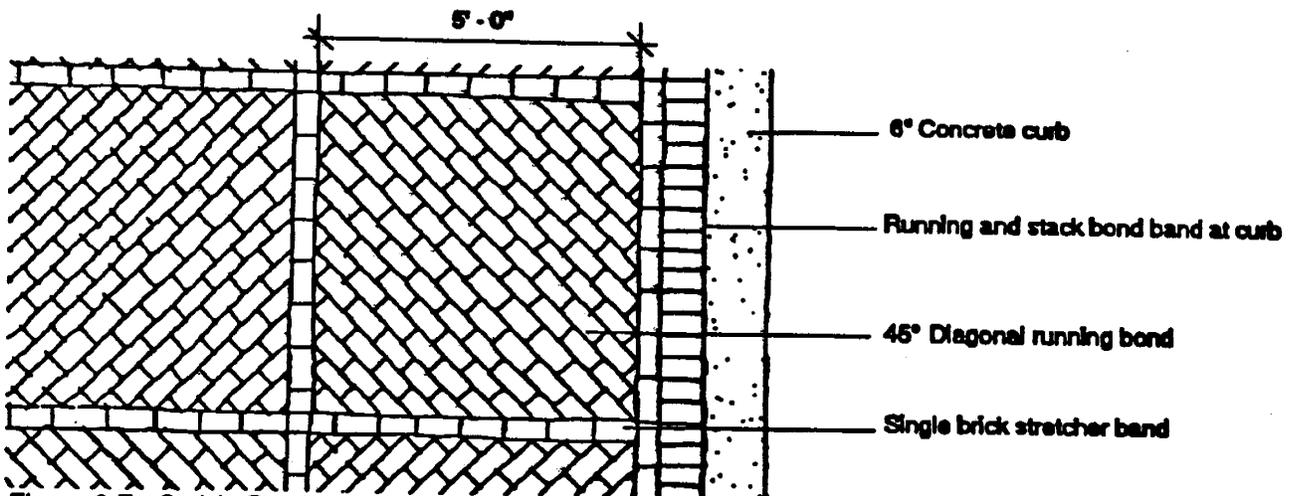


Figure 3-7: Carlyle Precinct Standard Sidewalk Paving in Square Area



PAVEMENT EDGES: Planter bed edges will be treated in several ways. Street trees along John Carlyle Street East and West shall be installed in five foot square grates which will fit within the five foot square modular grid. Street tree wells along Duke Street shall be flush with the pavement designed to match the ones at King Street Station . All other planters shall either be flush, at grade planters or have a seatwall enclosing them.

SPECIAL PAVING: In order to maintain a cohesive design connection of the streetscape to the square, special paving at major lobby entrances may not extend beyond the building zone.

3.5 STREET FURNITURE

3.5.1 SEATING

FORMAL SEATING- BENCHES: Formal seating shall be provided in an organized manner in Carlyle Precinct. Along the street at Blocks C, E, G, and H, a minimum of two backless benches shall be provided per block. These benches shall be located in the curb zone between the street trees alternating with the streetlights and shall be placed parallel to the street. Benches shall not be placed in front of residential entrances. The property owner shall provide the benches as required; they shall be maintained by the Carlyle Property Owners Association.

FENCES, GATES, COLUMNS AND WALLS: Fences, gates, columns and walls are prohibited except when they are used in conjunction with a residential entrance. Fences, gates and columns are restricted to the building zone.

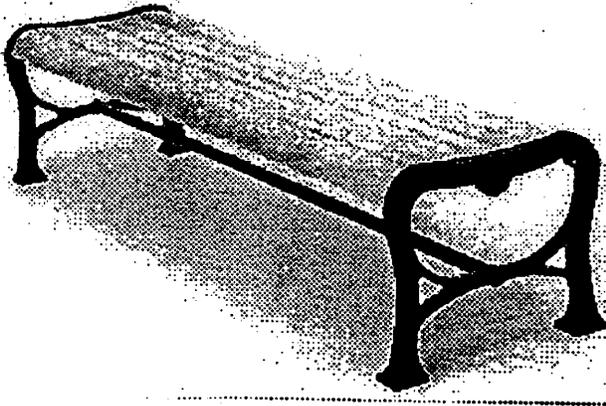


Figure 3-9: Backless Bench

3.5.2 STREET FIXTURES

TRASH RECEPTACLES: A minimum of four trash receptacles shall be provided along John Carlyle Street East and West at Blocks C and E. Two trash receptacles minimum shall be provided in the curb zone of Blocks G and H. One trash receptacle shall be located at each street corner; these may be counted towards the minimum requirements. Trash receptacles shall be located in the curb zone and next to streetlights where possible.



3.6 LIGHTING

3.6.1 STREETLIGHTS

Except for along Duke Street, double luminaire streetlights as specified in the General Streetscape Design Guidelines shall be used in Carlyle Precinct. The streetlights shall be spaced at sixty-four feet on center and shall be paired across the streets.

The axis of the double luminaire shall be aligned perpendicular to the curb.

Double luminaire streetlights shall be located at certain prominent points. At the "T" intersections of Jamieson Avenue and John Carlyle Street, a double streetlight shall be located at the top of the "T" aligned to the centerline of Jamieson Avenue; two paired double luminaire streetlights shall also be located at the point of tangency of the curb returns on Jamieson Avenue. At the north end of Carlyle Square, two double luminaire streetlights shall be aligned with the centerlines of each travel way perpendicular to those centerlines.

Along Duke Street, the "Gadsby" streetlight shall be used. The streetlights shall be spaced and paired to match the existing Gadsby Streetlights in King Street Station across Duke Street.

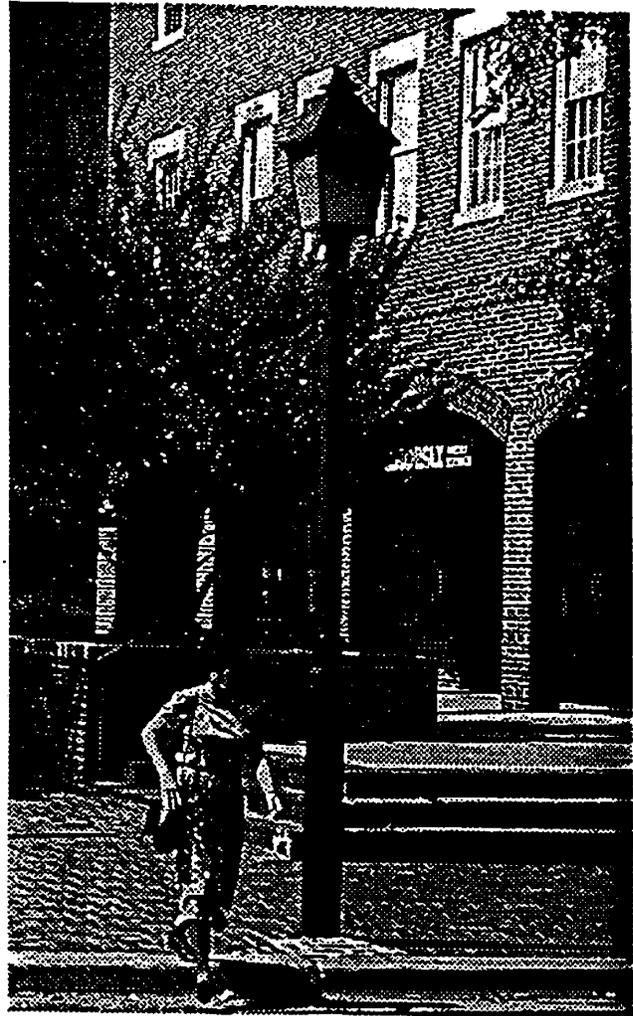


Figure 3-10: Gadsby Streetlight



3.7 PLANTING

3.7.1 STREET TREES

Except for along Duke Street, the street trees in Carlyle Precinct shall be *Quercus phellos*, Willow Oak. The Willow Oaks shall be spaced at thirty-two feet on center and shall be installed in tree grates. The tree grate shall be a five foot square, O.T. Series grate by Urban Accessories, Snohomish, Washington. Tree wells shall be fitted with permanent electrical outlets to allow for seasonal lighting displays.

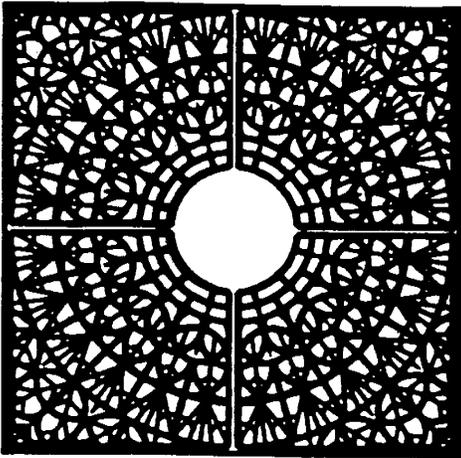


Figure 3-11: O.T. Series Tree Grate

Along Duke Street, the street tree shall be *Acer rubrum* 'Red Sunset', Red Sunset Maple. These street trees shall be spaced to match the existing street trees across Duke Street at King Street Station. They shall be planted in tree planting wells to match those across Duke Street; tree wells should be planted with English Ivy for a groundcover.

3.7.2 PLANTING BEDS AND PLANTERS

Planting is not required in front of retail shops in Carlyle precinct, however, the use of pots and window boxes is encouraged. For all other uses, planting shall be provided for seventy five percent of the net plantable facade in the building zone. Plantings shall include seasonal color to add to the marketplace atmosphere of the precinct.

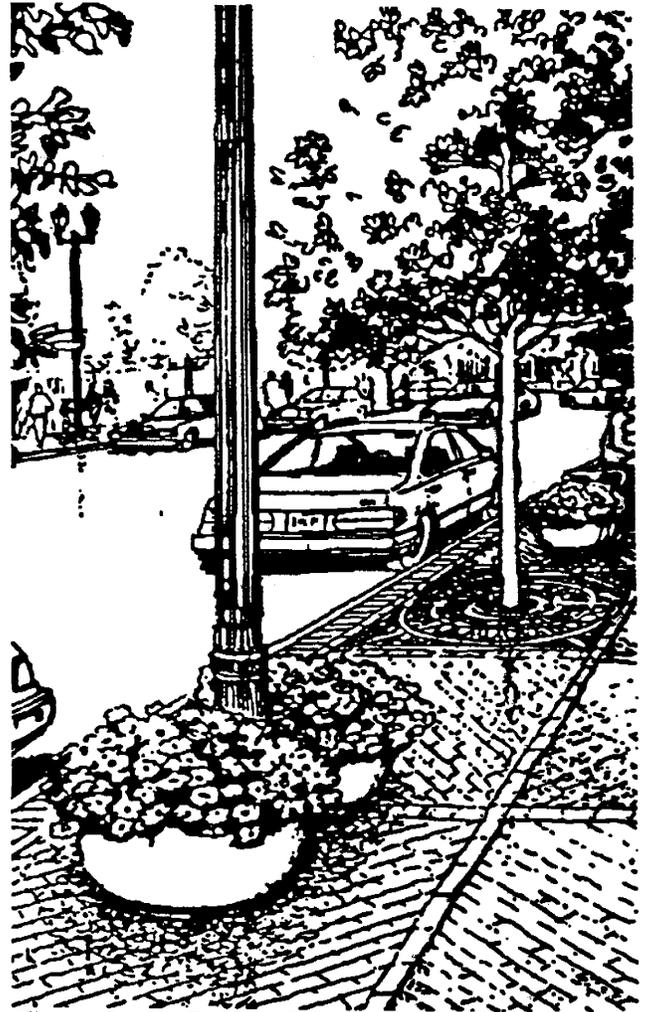


Figure 3-12: Pots at Streetlights

3.7.3 FREESTANDING PLANTERS

Two or three large pots of varying sizes shall be clustered together at the streetlight poles in Carlyle Precinct. The pots shall be the Lorraine series bowls by Magnalite Systems, Inc., Carson City, NV, catalog number LB3216-32" diameter, number LB3816-38" diameter, number LB3024-30" diameter and number 3630-36" diameter. The pots shall be the natural sand finish. Seasonal plantings in these pots will add unity and color to the streetscape; the seasonal plantings shall be installed and maintained by the Carlyle Property Owners Association.



Individual retail shops are encouraged to provide freestanding pots, hanging baskets and window boxes within the building zone of their shops. Planters shall be of a sufficient size and design to provide year round interest.

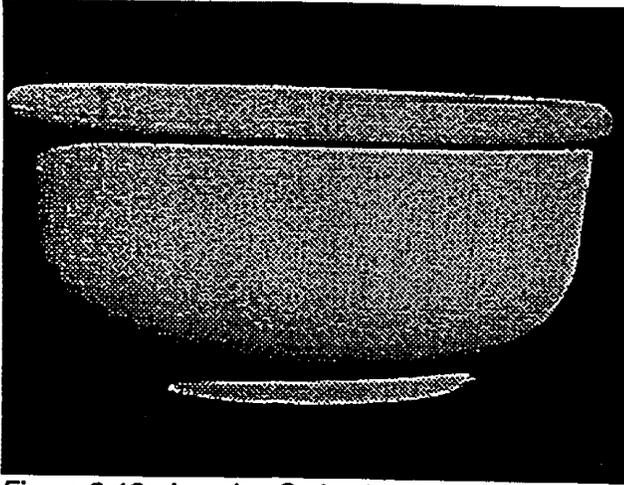


Figure 3-13: Lorraine Series Bowl

3.8 SIGNS

3.8.2 INFORMATION SIGNS

RETAIL AND PROFESSIONAL SIGNS: Retail shops in Carlyle Precinct are encouraged to use individual custom overhanging signs.

DIRECTORIES/KIOSKS: Three directories shall be provided in Carlyle Precinct. One shall be located at the Carlyle Square ends of each pedestrian through block connector in Blocks C and E. One shall be located in the north end of Carlyle Square.

CARLYLE MONUMENTS: A sign monument for Carlyle may be provided at John Carlyle Street and Duke Street. This monument shall be custom designed for this location and shall be located so as not to obstruct safe pedestrian or vehicular circulation.



Figure 3-14: Overhanging Retail Signs



3.10 ARCHITECTURAL PROJECTIONS

3.10.1 AWNINGS/CANOPIES

Custom designed awnings shall be provided for each retail shop in a color and design acceptable to Carlyle Development Corporation.

3.10.2 PORTE COCHERE

Porte Cocheres are prohibited in Carlyle Precinct except for Blocks C and E. The use of a marquee in conjunction with a theater is encouraged.

3.10.3 STEPS AND STOOPS

Steps and stoops may be used where needed. Consideration should also be given to recessed retail doorways.



Figure 3-15: Awnings



Figure 3-16: Marquee

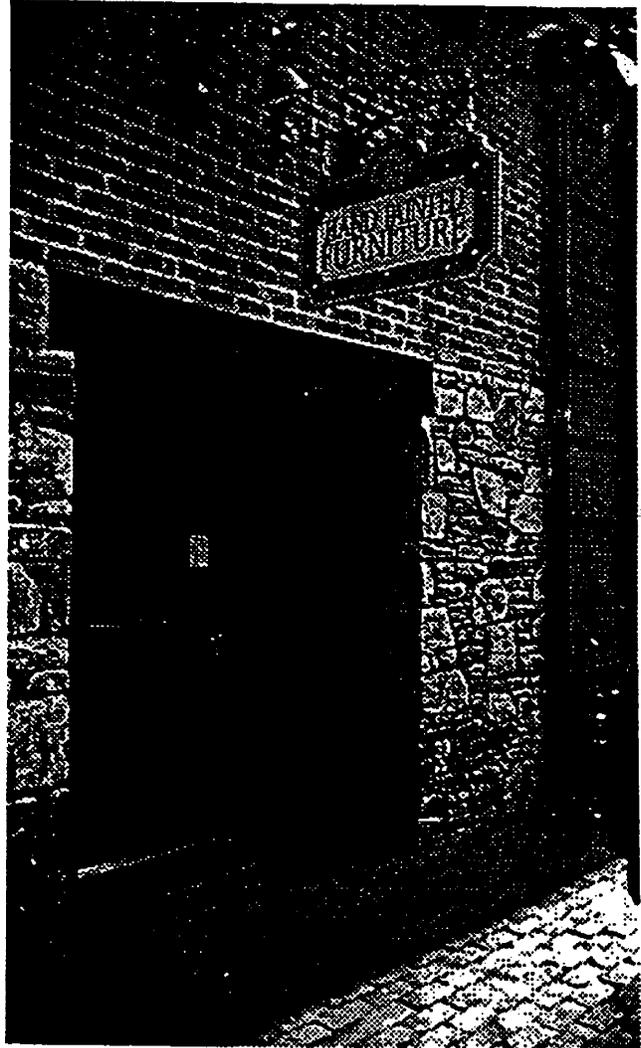


Figure 3-17: Recessed Doorway

3.11 TRANSITIONAL AREAS

3.11.2 TRANSITIONS BETWEEN PRECINCTS

BALLENGER PRECINCT TRANSITION: Carlyle Precinct is the dominant precinct. The standard sidewalk layout and paving shall change from Carlyle Precinct standards to Ballenger Precinct standards at the line created by the extension of the build-to line across John Carlyle Street. The street tree shall be as specified for the respective streets. The planting requirements for the building zone shall change at the same point as the sidewalk paving and layout.

TRANSITION TO DULANY PRECINCT: The transition from Carlyle Precinct to Dulany Precinct shall occur at the extension of the semicircular build-to-line at Block C extended perpendicular to Duke Street. Dulany Precinct is the dominant precinct and since the transition occurs at the Crescent, it shall be integrally designed with the Crescent.

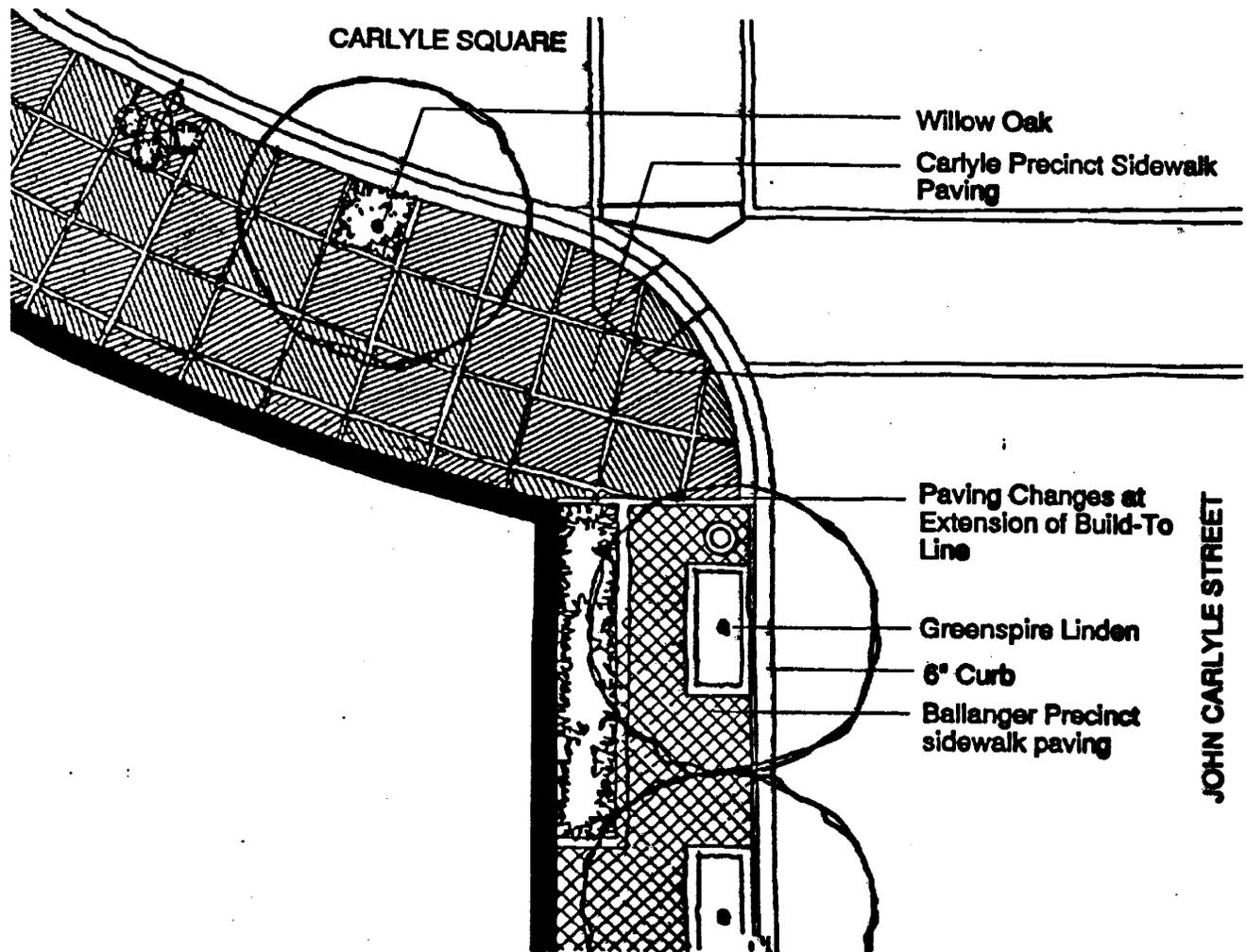


Figure 3-18: Ballenger Precinct Transition

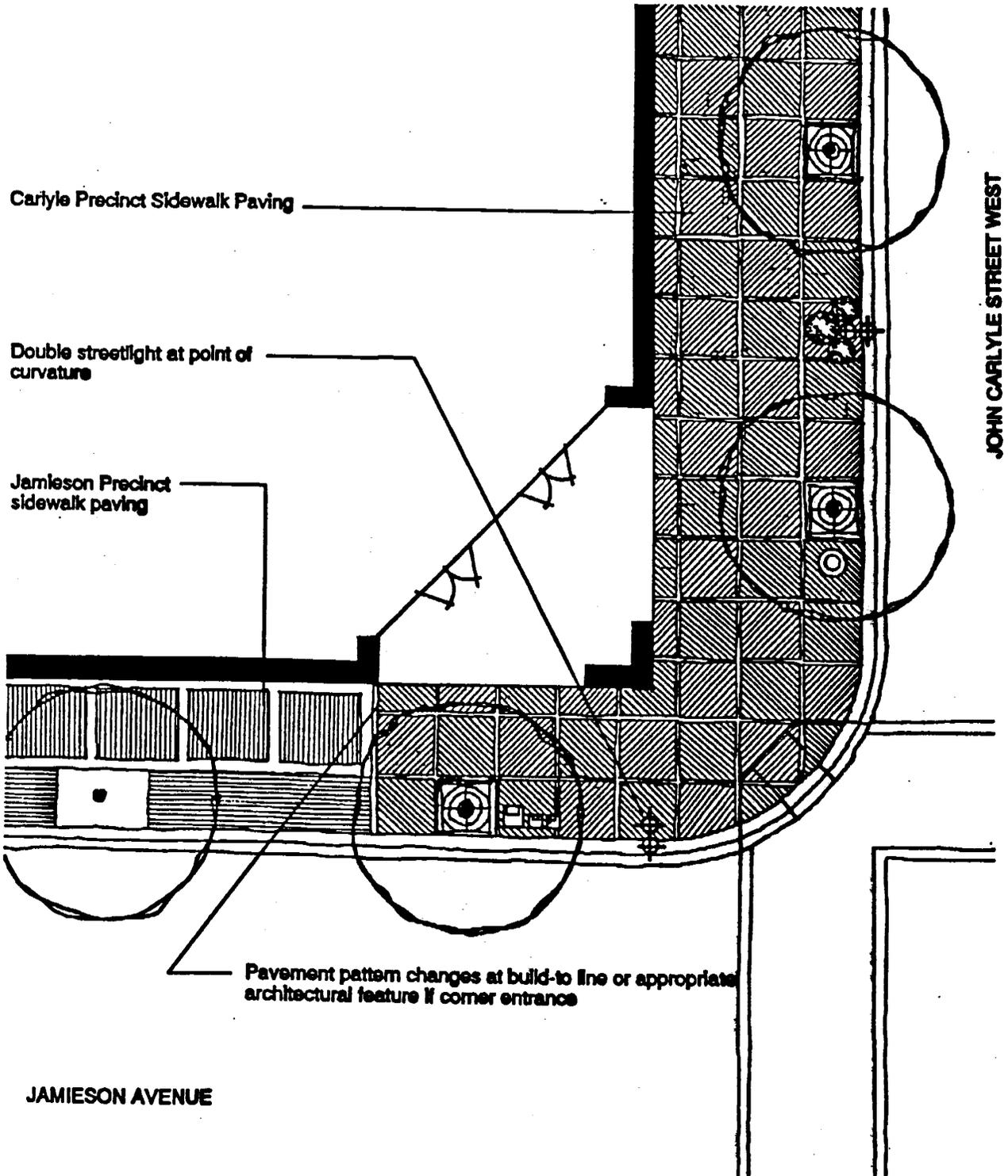


Figure 3-19: Jamieson Precinct Transition



JAMIESON PRECINCT TRANSITION: Jamieson Precinct is subordinate to Carlyle Precinct. The standard sidewalk layout and paving shall change from Carlyle Precinct to Jamieson Precinct standards at the line created by the extension of the build to line of Blocks C and E. The street trees are the same for each precinct. Planting for Carlyle Precinct shall extend around the corner into Jamieson Precinct for a minimum of ten feet ending at an appropriate architectural feature.

**3.11.3 TRANSITIONS TO AREAS
ADJACENT TO CARLYLE**

Carlyle Precinct is entirely within Carlyle and therefore no transitions are necessary.

4 DULANY PRECINCT

Additional information and special requirements for the streetscape for Dulany Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

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4 DULANY PRECINCT

Dulany precinct is the ceremonial spine of Carlyle—beginning with the Crescent at Duke Street and running through the Gardens to Eisenhower Avenue. It has the most continuous connected open spaces; the perception will be of a green street set within the urban fabric. The sequence of the spaces from the Crescent to the Gardens to Eisenhower Avenue is like a procession.

Dulany Precinct extends from Carlyle's northern edge at Duke Street through the center of Carlyle to the southern edge at Eisenhower Avenue.

4.1 DESIGN CONCEPT AND CHARACTER

4.1.1 CONTEXT

SITE PLAN: The Crescent forms a gateway for Dulany Precinct and Carlyle. The Crescent is created by the semicircular form of the buildings which embrace Duke Street. The spine created by Dulany Street and the Gardens contribute the main physical structure to the precinct. Buildings in Dulany Precinct are generally four, seven and nine stories; at Blocks F, G, M and N, four story portal elements create smaller scale gateways to the Gardens, reducing the height and mass of the buildings. The building edge is continuous along the street with only narrow breaks for the side streets and the required setbacks.

CIRCULATION: Dulany Precinct will be a major corridor for pedestrians, and vehicles traveling between Carlyle and the King Street and Eisenhower Metro Stations.

OPEN SPACE: The open spaces in Dulany Precinct are significant; there is generally a physical and visual connection between the buildings and the open spaces. Close proximity to these open spaces will enable easy access both visually and physically from automobiles and by pedestrians. There are two open spaces in Dulany Precinct: The Crescent and The Gardens. The Crescent is defined by semicircular seven to eight story buildings and is bisected by the medianed Dulany Street. The King

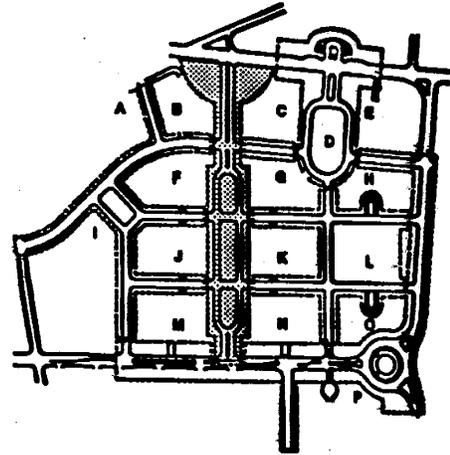


Figure 4-1: Dulany Precinct

Street Metro Station and the George Washington Masonic Memorial will be visible from the Crescent. The narrow three block long Gardens create a linear space at the center of Carlyle which is surrounded by architecture of a grand scale with emphasis on the main building entrances.

4.1.2 USE

The first floor uses in Dulany Precinct are office and hotel; the predominant use is office. Because of these uses, the activity in Dulany Precinct will be highest during the morning and evening rush hours with moderate use during midday lunch hours, evenings and weekends. The types of uses are private and semiprivate.



Figure 4-2: Mt. Vernon Place, Baltimore, Maryland

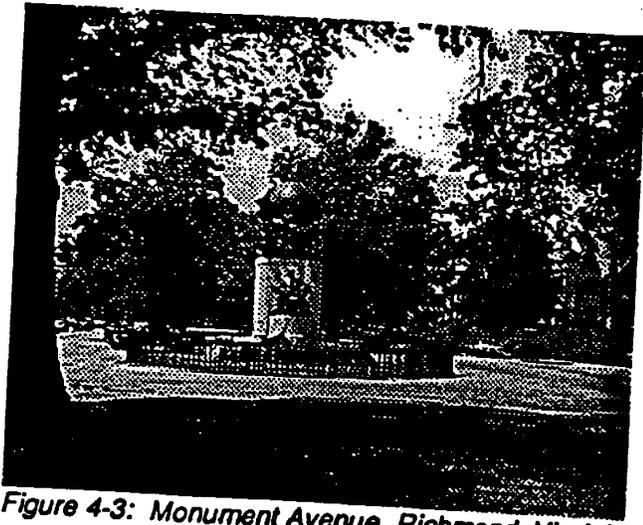


Figure 4-3: Monument Avenue, Richmond, Virginia

4.1.3 IMAGERY

Dulany Precinct is the ceremonial center of Carlyle; this is achieved by its formal site plan. The ceremonial aspects will be enhanced with rich materials in a simple, clean application. The streetscape will be open airy, elegant, lush and green. The formal nature of the Gardens will also enhance the ceremonial aspects of the precinct.

4.2 MAJOR OPEN SPACES

The two major open spaces in Dulany Precinct are The Crescent and the Gardens; these two open spaces are a primary part of the ceremonial character of Dulany Precinct.

4.2.1 THE CRESCENT

The Crescent is one of the largest spaces in Carlyle; it transitions from Duke Street funneling the views into Carlyle from the King Street Metro Station and the George Washington Masonic Memorial. The semi circular shaped buildings define a space that is part of Duke Street in addition to providing a

ceremonial entrance. The Crescent design shall be a "public art piece" and should project an image of Carlyle's traditional town planning and city design. The Crescent is the first space in the processional sequence of Dulany Precinct. The Crescent shall be formal and symmetrically balanced about the axis created by Dulany Street with landscape elements in the space that reinforce the Crescent form. The Crescent may be a location for a sculpture or Carlyle Sign Monument. It's design should use materials, such as brick, which are indicative of Carlyle's streetscape and buildings.

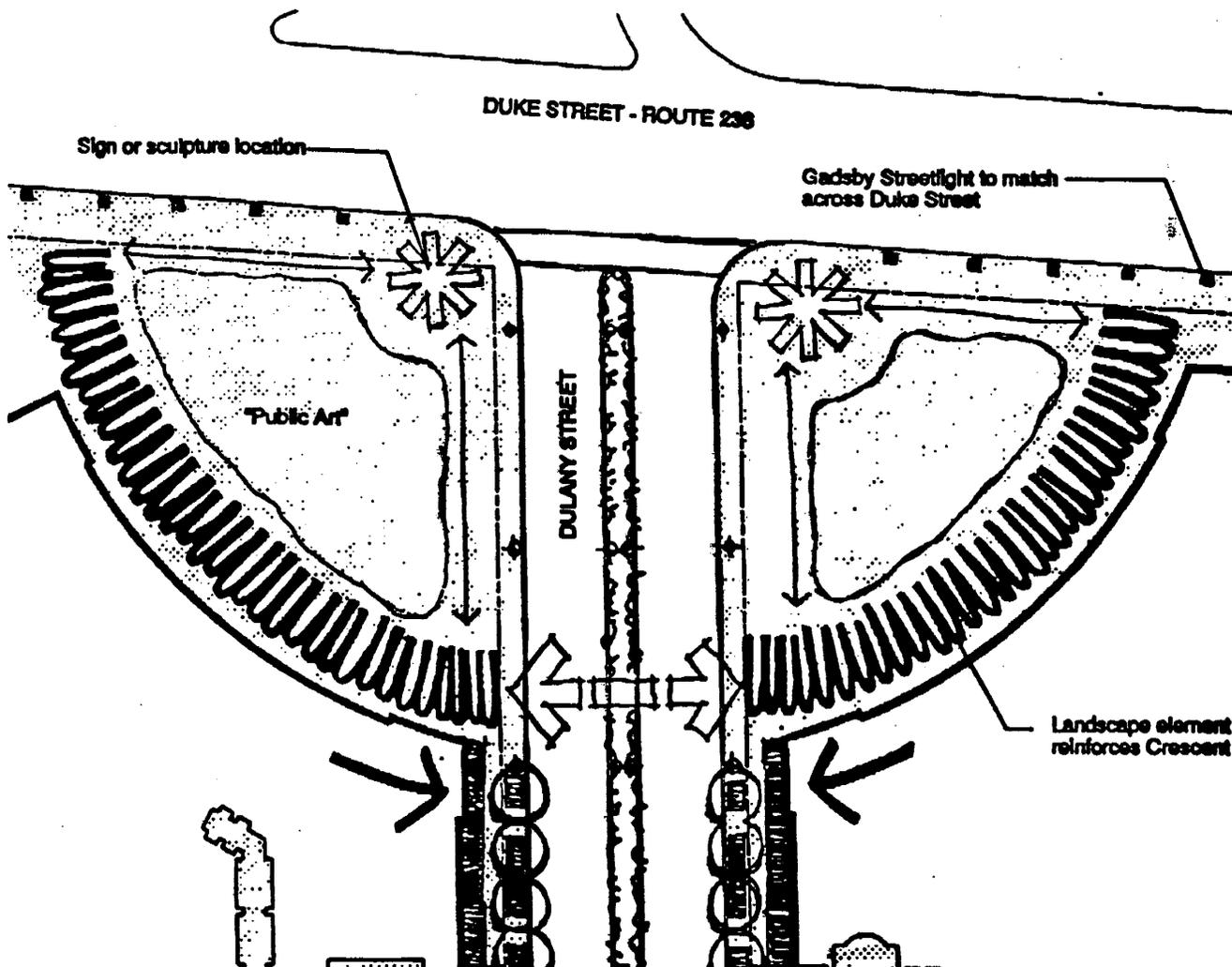


Figure 4-4: Crescent Open Space Concept



4.2.2 THE GARDENS

The Gardens continue over three blocks and are the focus of the precinct. They are oriented on the north-south axis of Dulany Street. Models for the Gardens include Mt. Vernon Place in Baltimore, Commonwealth Avenue in Boston, and Monument Avenue in Richmond. The Gardens should relate an English character, a green, well landscaped oasis, a soft space in contrast to the hardscape environs elsewhere in Carlyle. The Gardens will be a destination for pedestrians on a stroll and office workers during lunch; it will be a place for contemplation.

The Gardens shall be organized with a formal design having strong visual connections between them. They shall be balanced symmetrically about the north-south axis. Each of the three gardens should be distinct but similar. They will be linear in proportion and shall have an intimate scale. There shall be a clear hierarchy with the central garden having the most importance.

The sides of the Gardens along the north south axis shall form a semi-transparent edge to define the garden's interior spaces and reinforce the main north south axis. The axis shall be terminated in the upper and lower gardens by a focal element. The central garden shall be the primary focus; its design shall unify and connect the cross axis of the building entrances opposite each other. A landscape feature such as a fountain or statue shall be located at the intersection of the axes. Two minor cross axes are created by the major lobby entrances across the upper and lower gardens as well.

Fencing and columns, trees and plantings shall be used to articulate the Gardens' edge. Materials that may be used in the Gardens include granite or precast curbing and other elements, masonry walls, brick, metal fencing, sculpture, fountains and park pavilions.

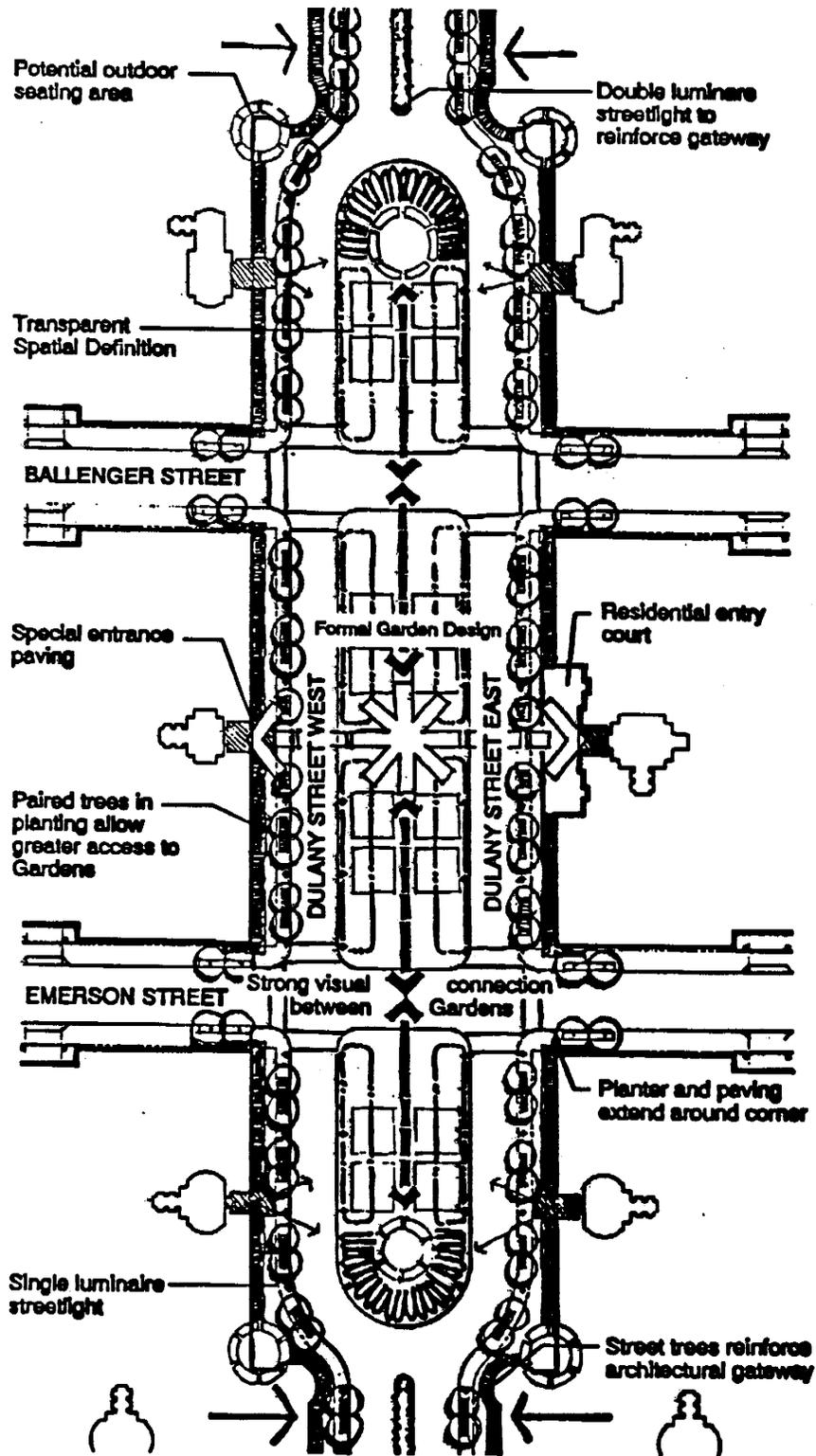


Figure 4-5: Gardens Open Space Concept



4.3 STREETSCAPE DESIGN

4.3.1 TYPICAL LAYOUT AND DIMENSIONS

There are two streetscape layouts for Dulany Precinct. One is for the Dulany Street East and West adjacent to the Gardens and the other is for the medianed portion of Dulany Street north and south of the Gardens.

GARDENS STREETS: For the streets along the Gardens, on the building side there shall be a eight foot, six inch curb zone with an eight foot, six inch pedestrian zone and an eight foot building zone. The curb zone will include a six inch curb with a one

foot brick band behind the curb. Street trees will be planted paired in tree planting wells surrounded by a six inch precast concrete curb; they will be seven feet wide and extend five feet from the street trees at each end. The pedestrian zone is next to the curb zone and has a one foot band with a six foot paving field and a second one foot band. Along the street, one foot bands shall extend perpendicular to the curb across the walk to the building zone at each end of the street tree planting wells.

On the Gardens side of the street, there shall be an eight foot wide walk having a six inch curb and a one foot band behind the curb, a five foot wide

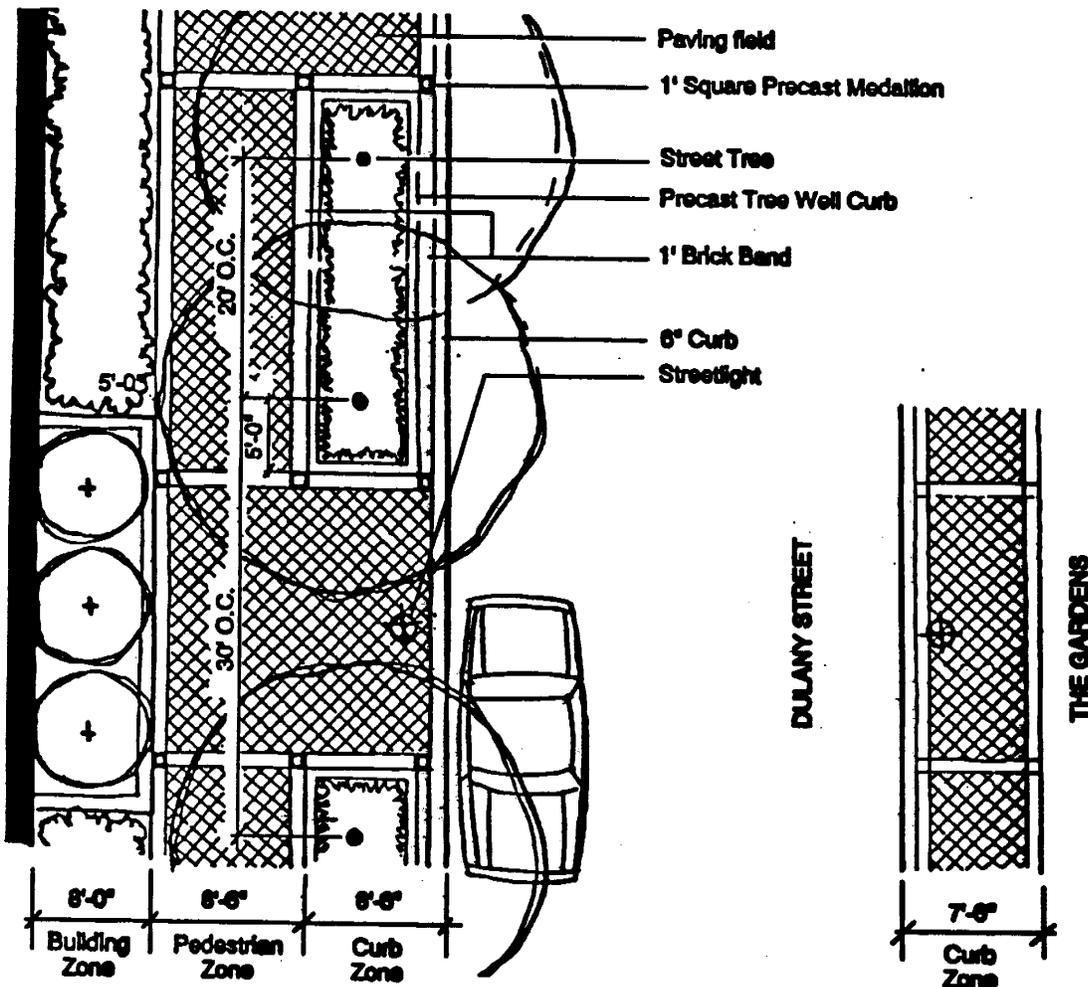


Figure 4-6: Dulany Precinct - Gardens Typical Streetscape



paving field and a one foot band along the edge of the walk. One foot bands perpendicular to the curb shall extend across the sidewalk matching the perpendicular bands across the street.

OTHER STREETS: The other streets in Dulany Precinct shall be treated in a similar manner as the streets adjacent to the Gardens, except that the trees shall be spaced uniformly at twenty feet on center paired in planting wells. The street has a twelve foot median which shall be landscaped and have double luminaire streetlights.

4.3.2 BUILDING ENTRANCES

MAJOR LOBBY ENTRANCE: If major lobby entrances are located centrally on the three Gardens as shown in the Block by Block Design Guidelines in the Development Plan, the street tree spacing may be interrupted on the streetscape to enhance and reinforce the cross axes of the Gardens.



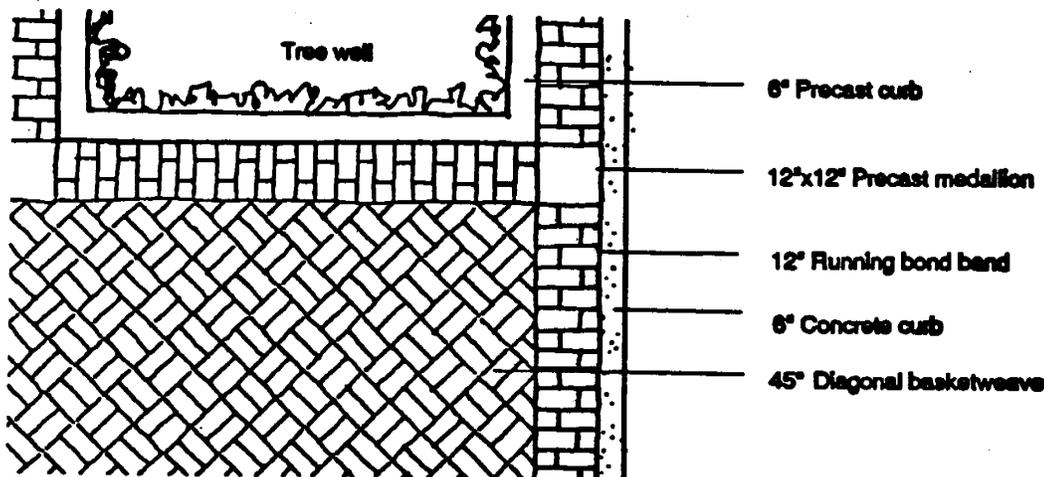
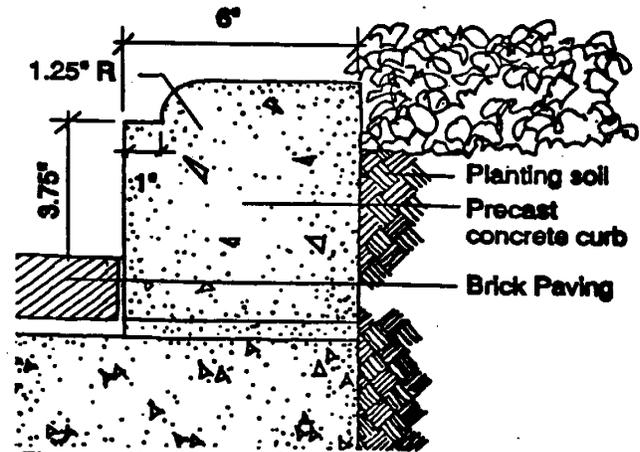
4.4 HARDSCAPE

4.4.1 PAVING

STANDARD SIDEWALK PAVING: The standard sidewalk paving for Dulany Precinct shall be the King William Range Brick Paver as specified in the General Streetscape Guidelines. The standard pattern shall be a diagonal basket weave laid at forty five degrees from the centerline of Dulany Street. Twelve inch banding shall be the same brick laid in a running bond pattern across the band. At the intersections of the bands, there shall be a twelve inch square precast concrete medallion. To achieve uniform coloring of the medallions, they shall be cast color #1006 by Maryland Cast Stone, Inc., Rockville, Maryland or equal approved by Carlyle Development Corporation. The medallion shall be finished with a light sandblast.

PAVEMENT EDGES: Planter bed edges shall be either curbed or enclosed with a seat wall.

The street tree planting wells shall have a six inch precast curb. To achieve uniform coloring of the street tree well curbing, the curbs shall be cast with color #1006 by Maryland Cast Stone, Inc., Rockville, Maryland or equal approved by Carlyle Development Corporation. The curbs shall be finished with a light sandblast.





4.5 STREET FURNITURE

4.5.1 SEATING

FORMAL SEATING - BENCHES: Benches are prohibited in the streetscape except at bus stops.

INFORMAL SEATING: Seat wall planters shall be provided for thirty-three percent of the net plantable building facades in the building zone. Planters will be evenly dispersed along the building street wall and shall be coordinated with other planters in the building zone.

4.5.2 STREET FIXTURES

TRASH RECEPTACLES: One trash receptacle shall be provided at each street corner on the east and west side streets at or beyond the point of curvature of the curb.

4.5.3 PUBLIC SERVICES

NEWSPAPER VENDING MACHINES: Newspaper vending machines shall be permitted at street corners on the east-west side streets.



4.6 LIGHTING

4.6.1 STREETLIGHTS

Streetlights in Dulany Precinct shall be spaced to match the two streetscape layouts. For the streets adjoining the Gardens, the streetlights shall be single luminaire streetlights; they shall be spaced at fifty feet on center and shall be paired across the street. For the rest of the streets in Dulany Precinct, the streetlights shall be spaced at eighty feet on center and paired across the streets. In the median, a double luminaire streetlight shall be used and single luminaire streetlights shall be used on each side of the street. Streetlights at all intersections along Dulany Street shall be located at the point of curvature of the curb returns so that they are paired across Dulany Street.



4.7 PLANTING

4.7.1 STREET TREES

The street tree for Dulany Precinct shall be the Redmond Linden, *Tilia americana* 'Redmond'. The trees shall be spaced to match the streetscape layouts. On the streets adjoining the Gardens, the street trees shall be paired at twenty feet on center with the pairs spaced at thirty feet apart. On the other streets in Dulany Precinct, the street trees shall be spaced at twenty feet on center.

4.7.2 PLANTING BEDS AND PLANTERS

Landscape plantings in Dulany Precinct streetscape shall be uniform and formal. Planting shall be provided in the building zone for one hundred percent of the net plantable facade area. The plantings shall be a minimum of five feet wide and may extend to the full width of the building zone.

LAWN: Lawn is prohibited in the building zone.

SHRUBS: Shrubs shall be massed in formal structured plantings. The use of clipped hedges and plants with architectural forms is encouraged. Shrubs which are acceptable for use in Dulany Precinct include: Hollies, Boxwood, Sweet Box, Compact Burning Bush, Azalea, Yew, Photinia, Cherry Laurels, and Pyracantha.

TREES: Trees which are acceptable in Dulany Precinct include European Hornbeam, Crape Myrtle, Dogwood, Fringe Tree, Star Magnolias, Higan Cherry and Stewartia.



4.10 ARCHITECTURAL PROJECTIONS

4.10.1 AWNINGS/CANOPIES

No awnings are permitted in Dulany Precinct.
Canopies may be used at major entrances.



4.11 TRANSITIONAL AREAS

4.11.2 TRANSITIONS BETWEEN PRECINCTS

JAMIESON PRECINCT TRANSITION: Jamieson Precinct is subordinate to Dulany Precinct. At Jamieson Avenue and Dulany Streets, the sidewalk paving shall change from Jamieson Precinct to Dulany Precinct standards at the extension of the build-to-line parallel to Dulany Street. The street trees shall be continuous along their respective streets. The building zone plantings on Dulany Street shall end at the build-to-line parallel to Jamieson Avenue. See Figure 4-9.

At Ballenger and Emerson Streets the street trees shall be continuous along the respective streets. Standard sidewalk paving will change from Dulany Precinct to Jamieson Precinct standards at the extension of the build-to line parallel to Dulany Street. The building zone plantings shall extend to the end of the side building wall for the building that faces onto Dulany Street. See Figure 4-10.

BALLENGER PRECINCT TRANSITION: Ballenger Precinct is subordinate to Dulany Precinct. The street trees shall be continuous along the respective streets. Standard sidewalk paving will change from Dulany Precinct to Ballenger Precinct standards at the extension of the build-to line parallel to Dulany Street. The building zone plantings shall extend to the end of the side building wall for the building that faces onto Dulany Street. See Figure 4-11.

EISENHOWER PRECINCT TRANSITION: Eisenhower Precinct is subordinate to Dulany Precinct. The two corners of Dulany Street and Eisenhower Avenue shall be treated symmetrically. See Figure 4-12. The area in front of the portal elements of Blocks M and N shall be designed to create small plazas in front of the building. The plazas shall be separated from the street edge and relate to the minor entrances at the portal elements. The standard sidewalk paving for Dulany Precinct shall extend to Eisenhower Avenue and the courtyard shall be paved with Dulany Precinct paving. The sidewalk paving shall change at the edge of the plaza to Eisenhower Precinct standard sidewalk paving.

The Dulany Precinct street trees shall extend to Eisenhower Avenue. There shall be three trees arranged in a quarter circle at each corner, with the one closest to Eisenhower Avenue aligning with the first row of trees along Eisenhower Avenue. The planters in the building zone for Dulany Precinct shall extend around the corner to Eisenhower Avenue a minimum of ten feet ending at an appropriate architectural feature. These planters shall be coordinated with the plaza design. Each plaza shall have at least one bench and one directory/kiosk within it.

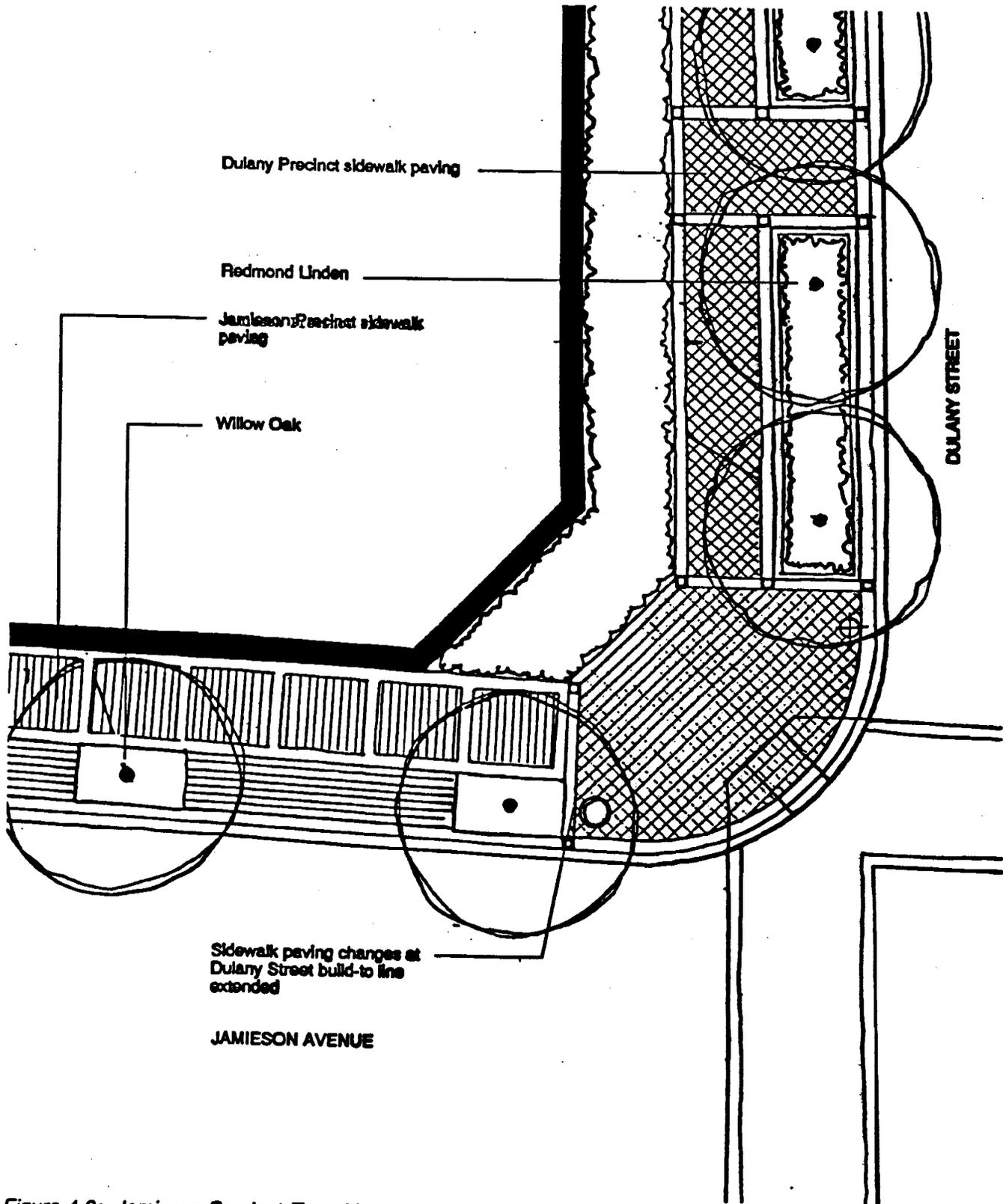


Figure 4-9: Jamieson Precinct Transition at Jamieson Avenue

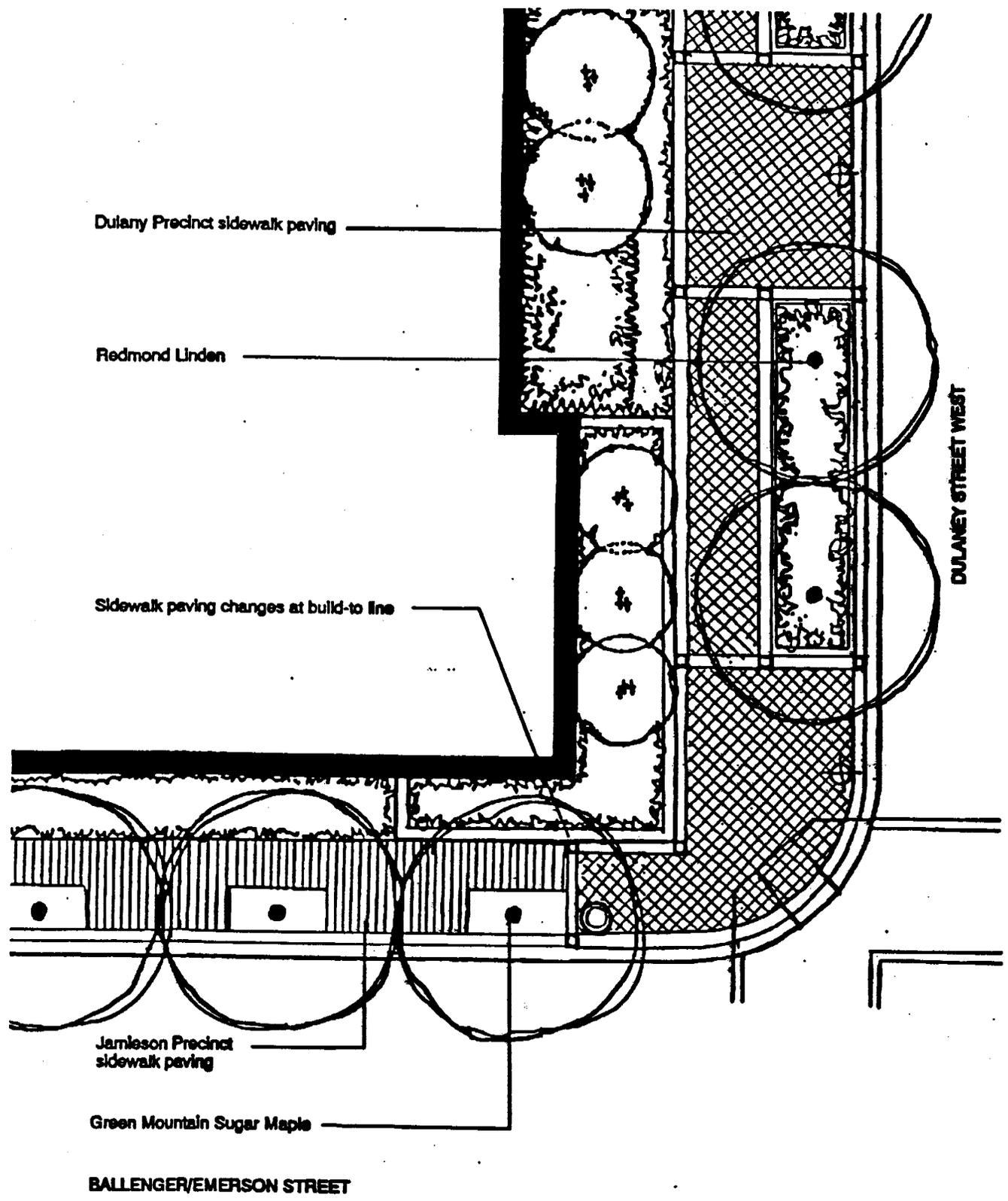
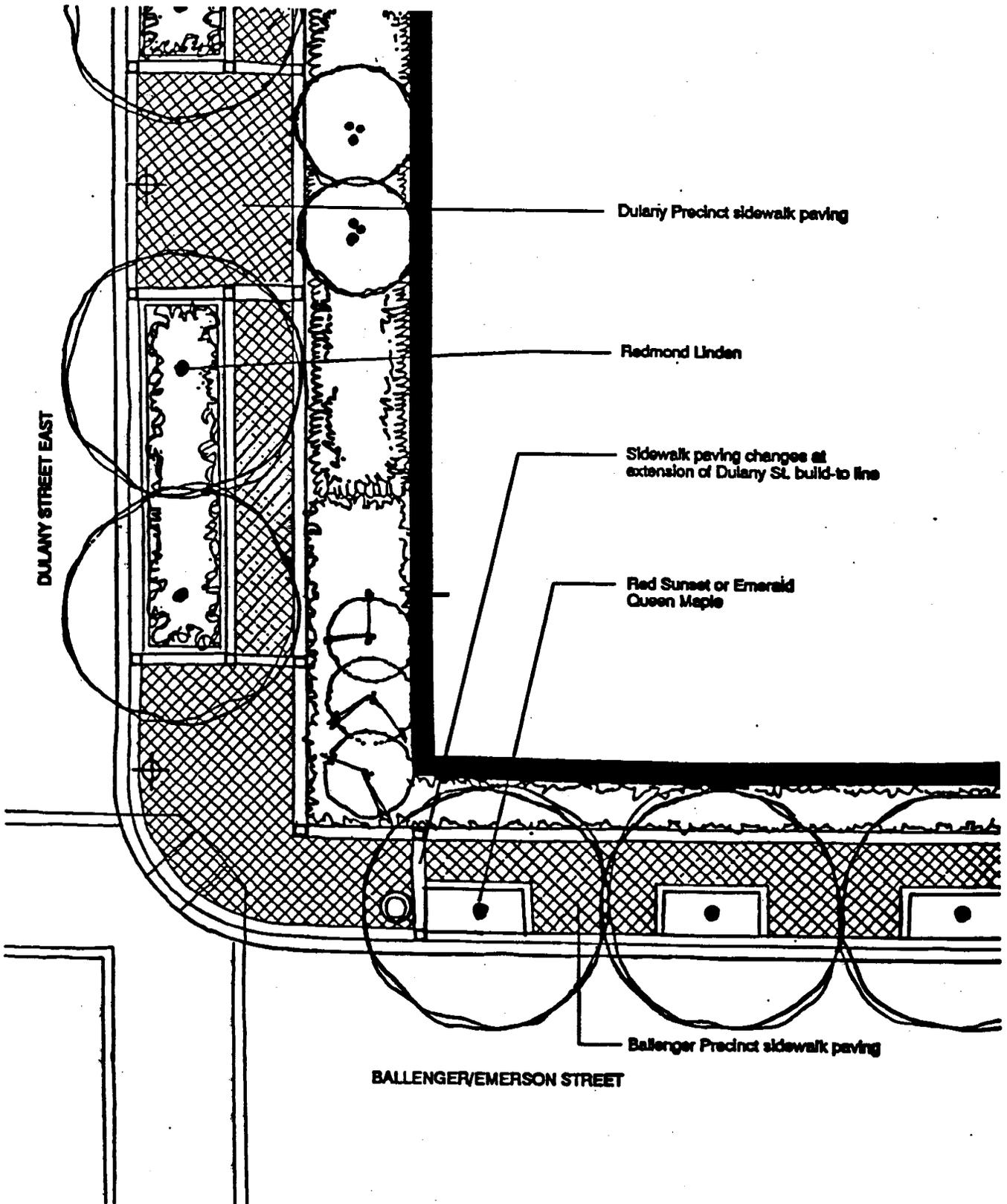


Figure 4-10: Jamieson Precinct Transition at Ballenger and Emerson Streets



4-11: Ballenger Precinct Transition

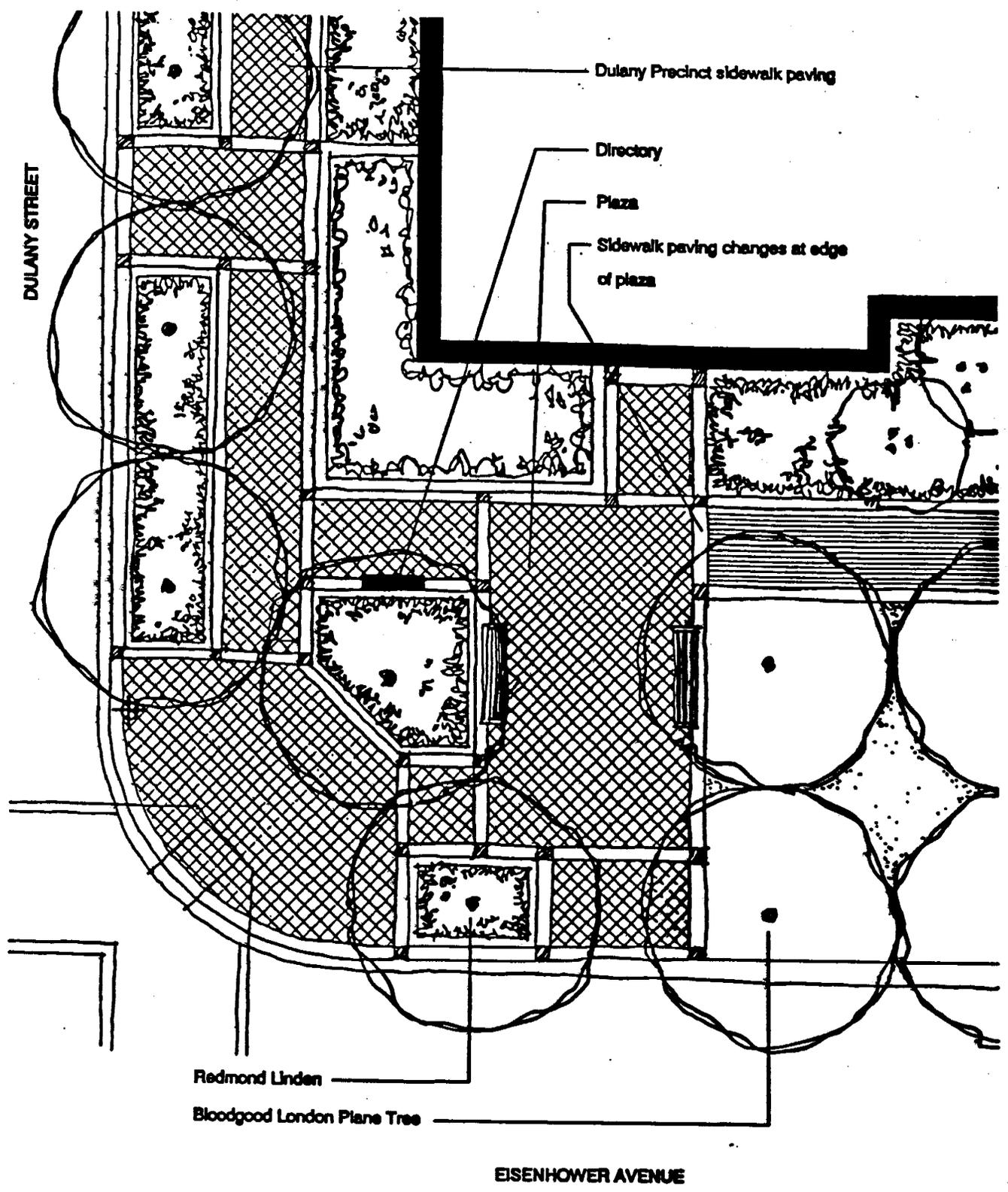


Figure 4-12: Eisenhower Precinct Transition

5 JAMIESON PRECINCT

Additional information and special requirements for the streetscape for Jamieson Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

<p>5.1 Design Concept and Character 5-3</p> <p>5.1.1 Context 5-3</p> <p>5.1.2 Use 5-3</p> <p>5.1.3 Imagery 5-4</p> <p>5.2 Major Open Space 5-5</p> <p>5.3 Streetscape Design 5-7</p> <p>5.3.1 Typical Layout and Dimensions 5-7</p> <p>•Jamieson Avenue</p> <p>•Courthouse Square</p> <p>•Other Streets</p> <p>5.3.2 Building Entrances 5-8</p> <p>•Major Lobby Entrance</p> <p>5.3.3 Vehicular Access Zones ... 5-9</p> <p>•Parking and Service Entrances</p> <p>•Drop Offs/Lay-Bys</p> <p>5.4 Hardscape 5-10</p> <p>5.4.1 Paving 5-10</p> <p>•Standard Sidewalk Paving</p> <p>•Special Paving</p> <p>•Planter Bed Edges</p> <p>5.5 Street Furniture 5-11</p> <p>5.5.1 Seating 5-11</p> <p>•Formal Seating: Benches</p> <p>5.5.2 Street Fixtures 5-11</p> <p>•Trash Receptacles</p> <p>•Fences, Gates, Columns and Walls</p> <p>•Bollards</p> <p>5.6 Lighting 5-12</p> <p>5.6.1 Streetlights 5-12</p> <p>5.6.3 Building Lights 5-12</p> <p>•Facade Lighting</p>	<p>5.7 Planting 5-13</p> <p>5.7.1 Street Trees 5-13</p> <p>5.7.2 Planting Beds and Planters 5-13</p> <p>5.7.3 Free Standing Planters 5-13</p> <p>5.8 Signs 5-14</p> <p>5.8.2 Information Signs 5-14</p> <p>•Retail and Professional Signs</p> <p>•Directories/Kiosks</p> <p>5.10 Architectural Projections 5-15</p> <p>5.10.1 Awnings/Canopies 5-15</p> <p>5.11 Transitional Areas 5-16</p> <p>5.11.2 Transitions between Precincts 5-16</p> <p>•Dulany Precinct Transition</p> <p>•Carlyle Precinct Transition</p> <p>•Eisenhower Precinct Transition</p> <p>5.11.3 Transitions to Areas Adjacent to Carlyle 5-22</p> <p>•West side of Elizabeth Lane</p>
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5 JAMIESON PRECINCT

The focus of Jamieson Precinct will be the Alexandria Federal District Courthouse which will face onto Courthouse Square. The courthouse becomes very prominent in the civic image of Jamieson Precinct.

Jamieson Precinct forms the western edge of Carlyle. It encompasses the area from Duke Street to Eisenhower Avenue along the west side of Carlyle and is generally bounded by Dulany Precinct on the east, except where it continues along Jamieson Avenue to Holland Lane with an interruption at Carlyle Square.

5.1 DESIGN CONCEPT AND CHARACTER

Jamieson Precinct will significantly contribute to the variety of use and types of places in Carlyle. It will add a public, civic component not found elsewhere which will be its focal point the courthouse at Courthouse Square. Making up the balance of the precinct is perhaps the greatest complement of mixed use of all the precincts in Carlyle. Together, the civic component and greater level of mixed use will provide a strong urban character.

5.1.1 CONTEXT

SITE PLAN: Jamieson precinct will consist of apartment and office towers with office buildings on the side streets. There will also be retail on the ground floors of the buildings along Jamieson Avenue and possibly facing onto Courthouse Square; this retail will be supplemental to the other uses in both the nature of the retail use and the its expression in the architecture. As stated previously, the focal point of the precinct will be the courthouse which faces onto the square. The long broad curve of Jamieson Avenue will present ever changing vistas to the pedestrian and driver as they move along it.

CIRCULATION: Jamieson Avenue will probably be a thoroughfare for drivers within Carlyle because of the connection to Mill Road and the Eisenhower Valley. Automobile circulation in the rest of

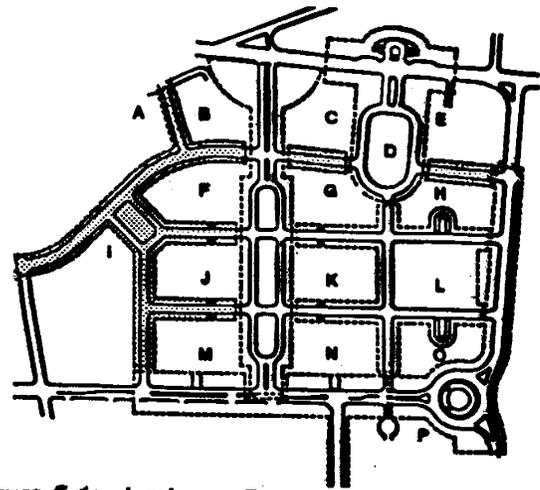


Figure 5-1: Jamieson Precinct

Jamieson Precinct will be local traffic with people who have their destination in the precinct. Courthouse Square will get a great deal of pedestrian traffic especially at lunch time as will Jamieson Avenue leading to the shops at Carlyle Square and King Street Station.

OPEN SPACE: In Jamieson Precinct the open space is Courthouse Square. This small square is located off Jamieson Avenue and is bounded by streets on all sides. The streetscape around the square should be treated as part of the square since the views in the space will be very contained by the height and mass of the surrounding buildings.

5.1.2 USE

Jamieson Precinct is perhaps the most mixed use precinct of all the precincts of Carlyle. The predominant use in Courthouse Square is office with the major first floor use being retail. The street level will have a pedestrian interaction through the relationship of the retail and the street. The use of the streetscape will be heavy during the day with peak use at lunch time by office workers going to lunch, walking to nearby shopping and strolling. The uses in Jamieson Precinct will be a mix of public uses such as the courthouse and retail shops, semi-public uses such as the offices and private uses such as the residences.

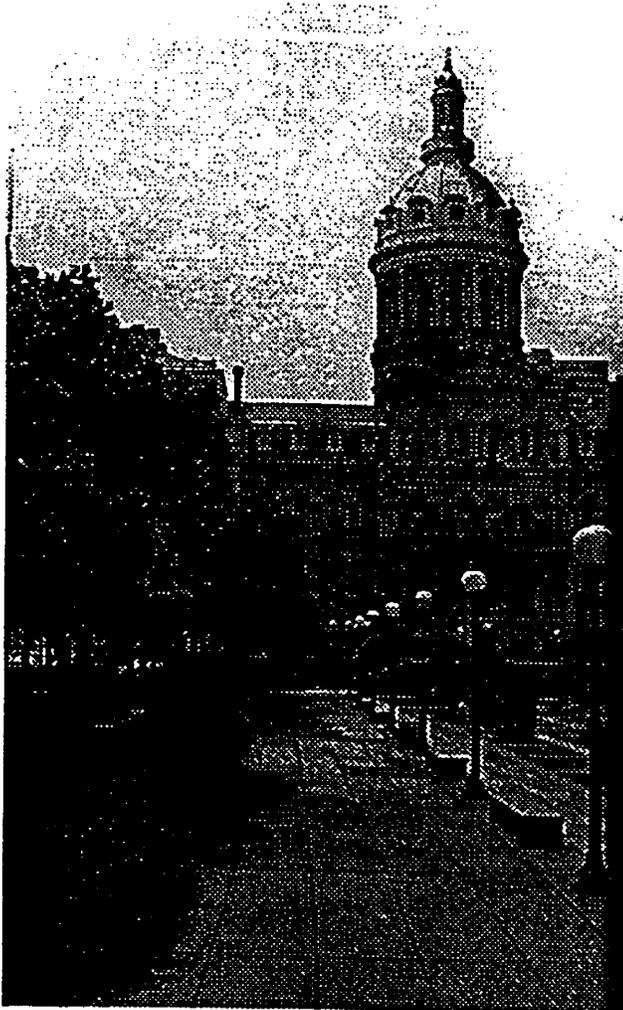


Figure 5-2: War Memorial Plaza/Baltimore City Hall

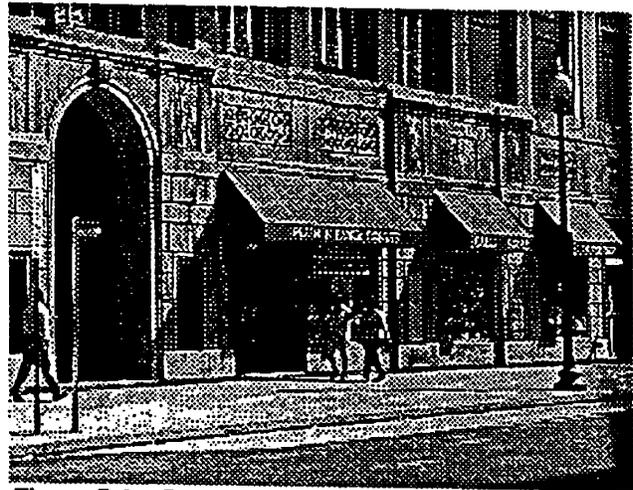


Figure 5-3: Downtown Washington, D.C.

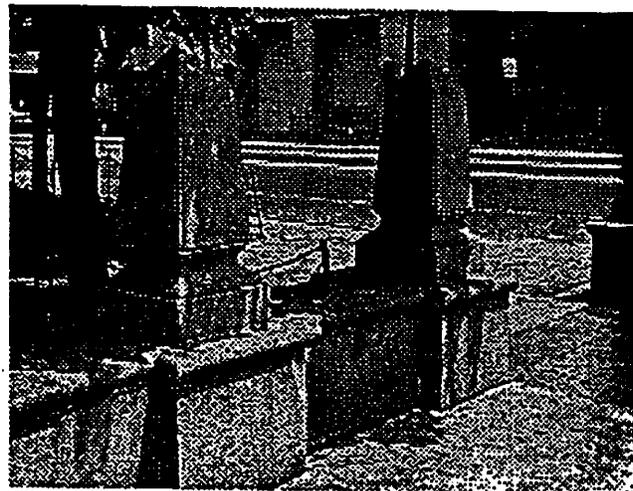


Figure 5-4: Civic Bollards

5.1.3 IMAGERY

The image that Jamieson Precinct projects is that of a civic place such as War Memorial Plaza and the City Hall in Baltimore. The federal courthouse and the adjoining square give the feel of an authoritative place. The buildings in Jamieson Precinct will give a sense of ordered grandeur and the scale of the streetscape should be monumental yet peaceful. The square should express a sense of order and authority.

5.2 MAJOR OPEN SPACE

The single open space in Jamieson Precinct is Courthouse Square. This square was described in the Design Report and Development Plan as:

... a shaded paved plaza.... The Square takes on the scale of Savannah's Squares, Johnson Square, in particular. This square will be framed by mid-rise office and residential buildings. ...Unit paver brick, precast or granite cobble, is being explored for the floor of the plaza. This patterning will be repeated on the adjacent 20' wide sidewalk areas. Seating areas will be clustered under a grove of ginkgo trees. It is hoped that a sculpture garden can be

incorporated under the trees as well. The court's principal feature will be a fountain marking the intersection of the many street axes which meet here.

Courthouse Square should be monumental in scale representing a grand gesture that supports the stature of the courthouse. The square should be a stately place; it should present an image of an urban place with a focal element which supports the civic nature of the precinct similar in character to the Grant Memorial in Washington.

The square is defined by the tall buildings on the

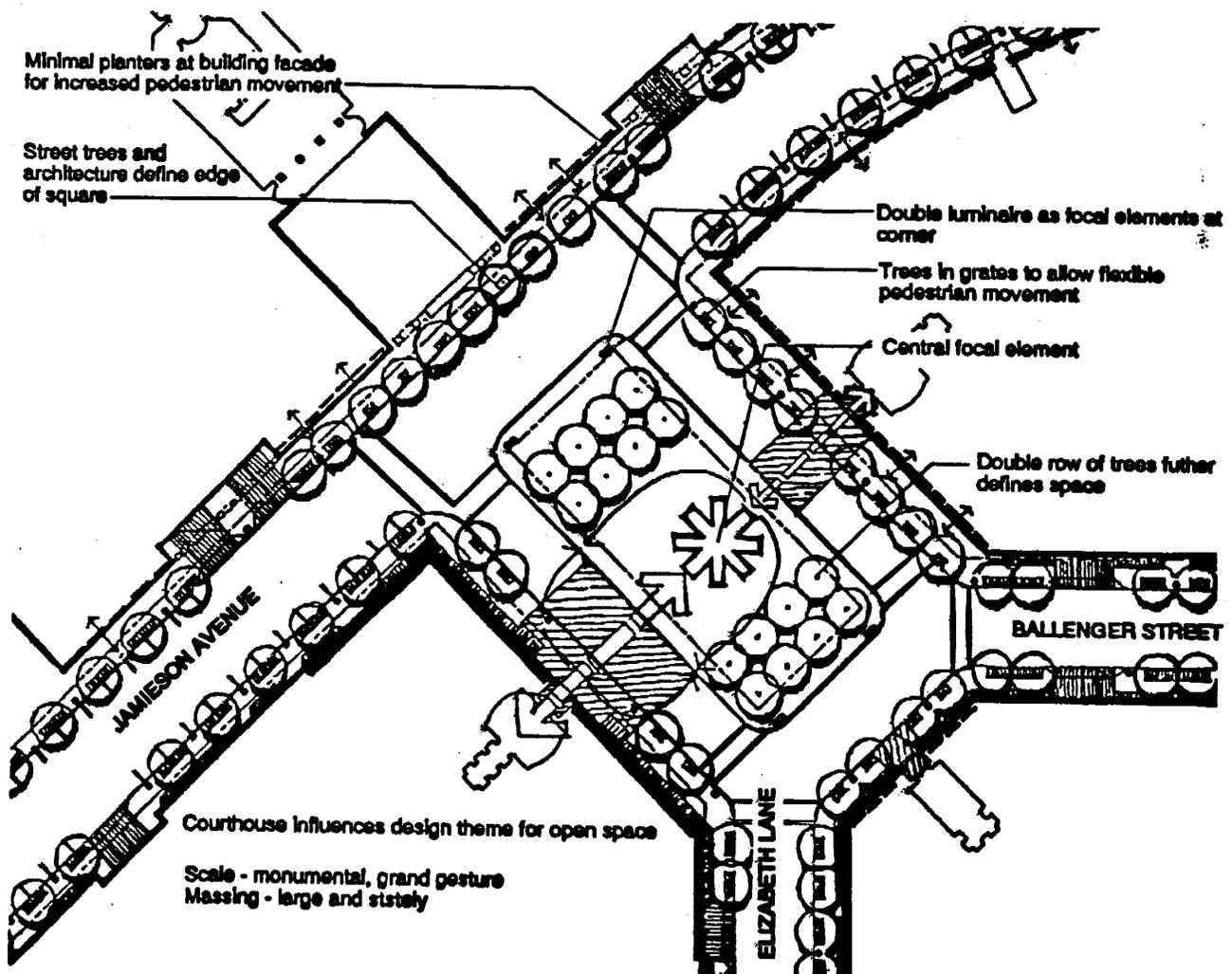


Figure 5-5: Courthouse Square Open Space Concept



four sides. The design for the square shall recognize and include the streetscapes along the buildings as part of the square. The interior edge of the square is not required to be treated as the typical streetscape for Jamieson Precinct and shall be designed as an integral part of the square to encourage flexible movement of people into the square. The use of tree grates in the streetscape around the square encourages this as well.

In order to emphasize the importance of the courthouse to Jamieson Precinct, the design of the square should focus on the courthouse. The primary axis of the square will be the cross axis which aligns to the courthouse. Visual and physical connections to the buildings at either side of the square shall be provided on this axis with emphasis given to the relationship with the courthouse building. The long axis of the square shall be secondary; no direct physical access shall be provided on this axis, although a visual connection may be provided. The middle portion of the square shall be a predominantly paved plaza with ample formal seating. This plaza shall focus on a central focal element such as a monument or a fountain. The ends of the square shall be planted with trees to provide a shady respite and to emphasize the courthouse axis. A double luminaire streetlight shall be provided at each interior corner of the square.

5.3 STREETScape

5.3.1 TYPICAL LAYOUT AND DIMENSIONS

There are three similar streetscape treatments for Jamieson Precinct depending on the location within the precinct. These treatments are for Jamieson Avenue, Courthouse Square, and the remainder of the streets in Jamieson Precinct. Where two streetscape treatments intersect, they shall be designed to fit together and not change abruptly.

with a five foot wide by nine foot long flush street tree well. The building zone shall be a maximum of two feet, six inches wide and the pedestrian zone shall be flexible to take up the remaining width of the sidewalk. The pedestrian zone and the building zone shall be paved in a single six and a half foot by seven foot module with a one foot band around the inside edge of the module.

JAMIESON AVENUE: For all of Jamieson Avenue, the curb zone shall be six feet, six inches wide and shall have a one foot brick band behind the curb

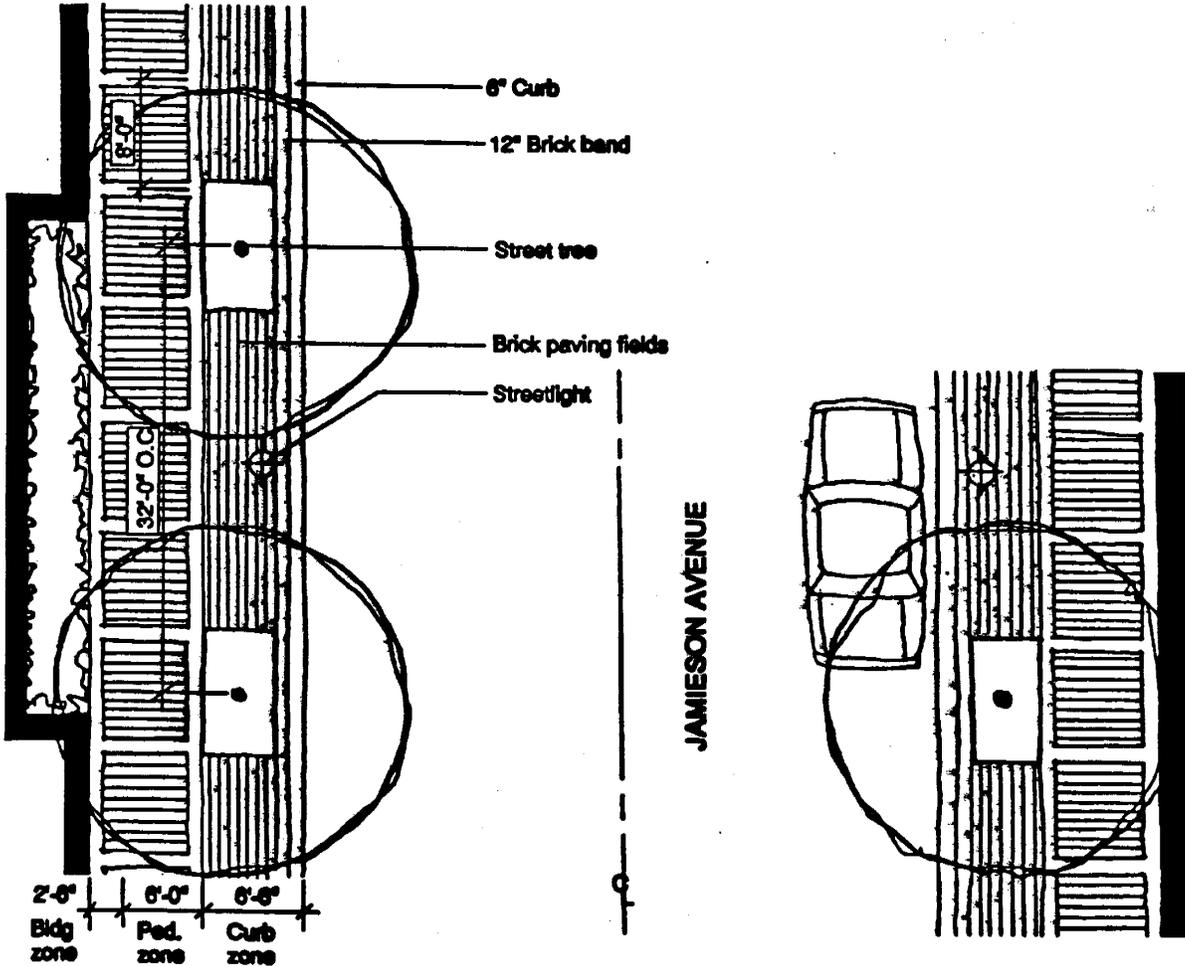


Figure 5-6: Jamieson Avenue - Typical Streetscape Layout

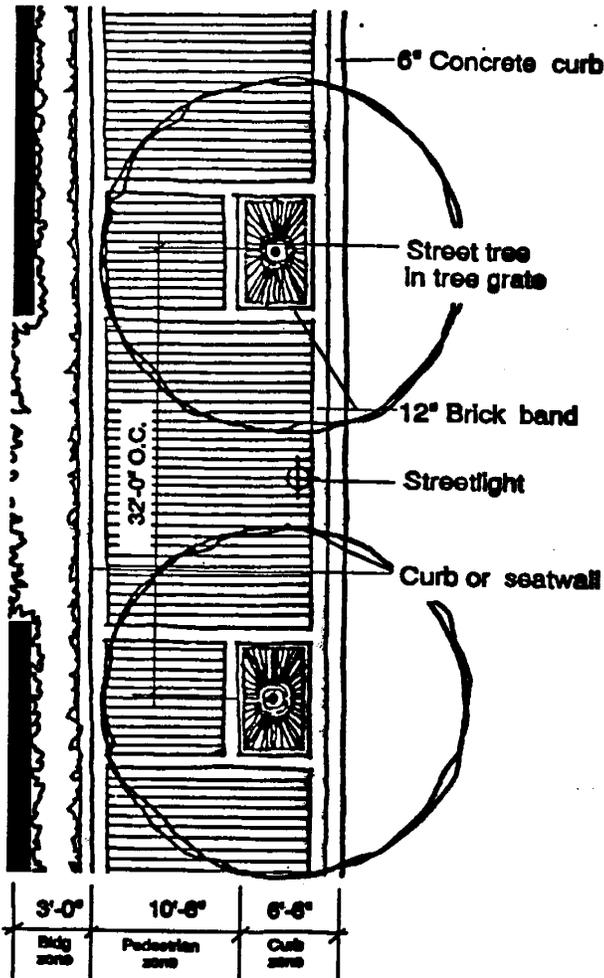


Figure 5-7: Courthouse Square - Typical Streetscape

COURTHOUSE SQUARE: The streetscape surrounding Courthouse Square shall have a six foot, six inch wide curb zone with a one foot band behind the curb. Street trees shall be installed in a five foot by eight foot tree well with tree grates as specified in Section 5.7.1 - Street Trees. The pedestrian zone shall be ten feet, six inches wide; the building zone shall be three feet wide and shall contain plantings as required in Section 5.7 - Planting. At each end of the tree grate there shall be a one foot brick band extending across the walk perpendicular to the curb. A one foot band shall also connect these two bands along the tree grate.

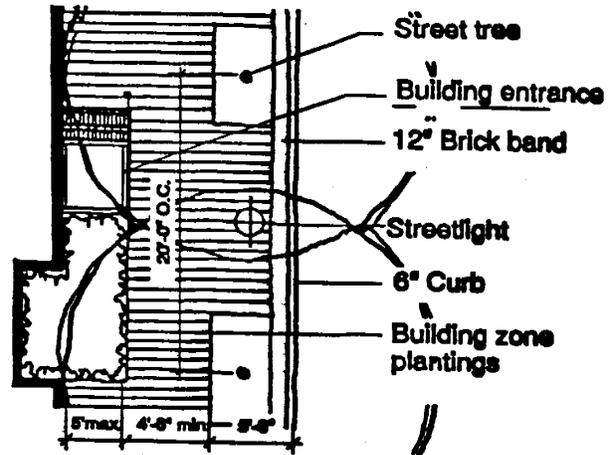


Figure 5-8: Other Streets in Jamieson Precinct - Typical Streetscape

OTHER STREETS: All other streets in Jamieson Precinct shall have a five and one-half foot curb zone with a one foot band running along the curb and a four foot wide tree well opening. The street tree well length shall be determined in eight foot modules. The pedestrian zone and the building zone shall be flexible to accommodate stoops, steps, building entrances, and planters. The pedestrian zone shall be four and one half feet wide minimum and the building zone shall be five feet wide maximum. Both shall be paved in a single simple paving with no banding.

5.3.2 BUILDING ENTRANCES

Building entrances shall conform to the requirements in the General Streetscape Guidelines and the following additional requirements:

MAJOR LOBBY ENTRANCE: Major lobby entrances facing onto Courthouse Square are encouraged to extend special paving to the curb, beyond the right of way. This will reinforce the strong relationship of the buildings to the square. Any special paving within the right of way will require approval by the City of Alexandria; and, the maintenance shall be the responsibility of the parcel owner.

5.3.3 VEHICULAR ACCESS ZONES

The requirements of the General Streetscape Guidelines shall apply with the following additions:

PARKING AND SERVICE ENTRANCES: For all parking and service entrances in Jamieson Precinct, the one foot brick band along the curb shall turn and delineate the edge of the parking and service entrance. The band shall extend to the building facade.

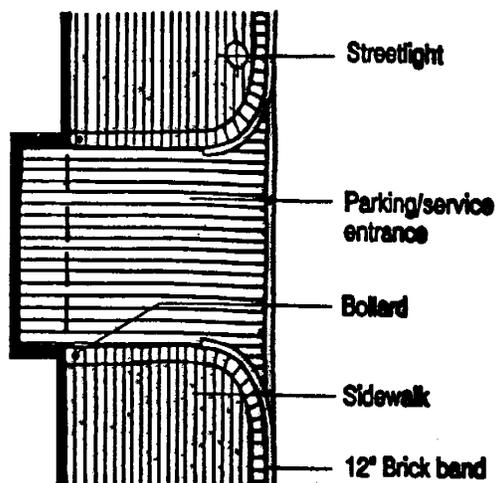


Figure 5-9: Banding at Parking and Service Entrance in Jamieson Precinct

DROPOFFS/LAYBYS: At the portion of Block A facing Courthouse Square, the use of a dropoff is encouraged; however, the rhythm and massing of the streetwall and streetscape shall be minimally interrupted. This may be achieved with an arcade or gateway wall which frames a courtyard.



5.4 HARDSCAPE

5.4.1 PAVING

STANDARD SIDEWALK PAVING: The standard sidewalk paving for Jamieson Precinct shall be the King William Range brick paver. All banding shall be twelve inches wide in a single basket weave pattern.

The paving fields will vary according to their location. Along Jamieson Avenue, within the rectangular modules in the pedestrian zone, the brick pavers shall be laid in a running bond pattern perpendicular to the curb. In the curb zone along Jamieson Avenue, the paving shall be a running bond laid parallel to the curb. For the Courthouse Square streetscapes, the paving shall be a running bond laid perpendicular to the curb in both the curb and pedestrian zones. For all other streets in Jamieson Precinct, the standard sidewalk paving pattern shall be a running bond perpendicular to the curb in all zones.

SPECIAL PAVING: For those buildings with lobby entrances on Courthouse Square which use special sidewalk paving, the special paving shall relate to the paving of Courthouse Square in addition to the building architecture.

PLANTER BED EDGES: Street tree wells shall be flush with grade in Jamieson Precinct except around the square where tree grates shall be used. See Section 5.7.1 - Street Trees for the grate specification. All planters in the building zone shall at a minimum have a curbed edge. Raised planters of seatwall height are acceptable as an alternative to this requirement.

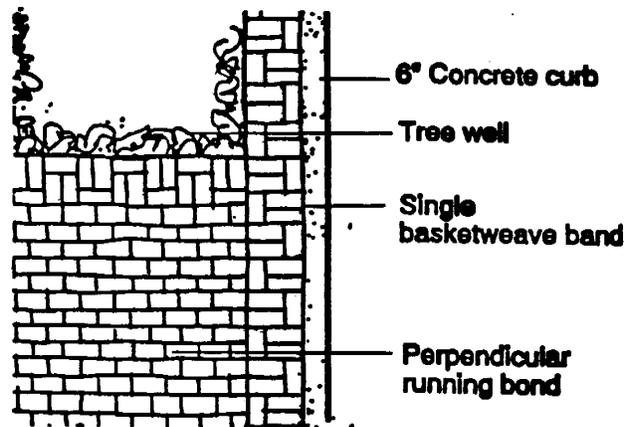


Figure 5-11: Courthouse Square Sidewalk Paving

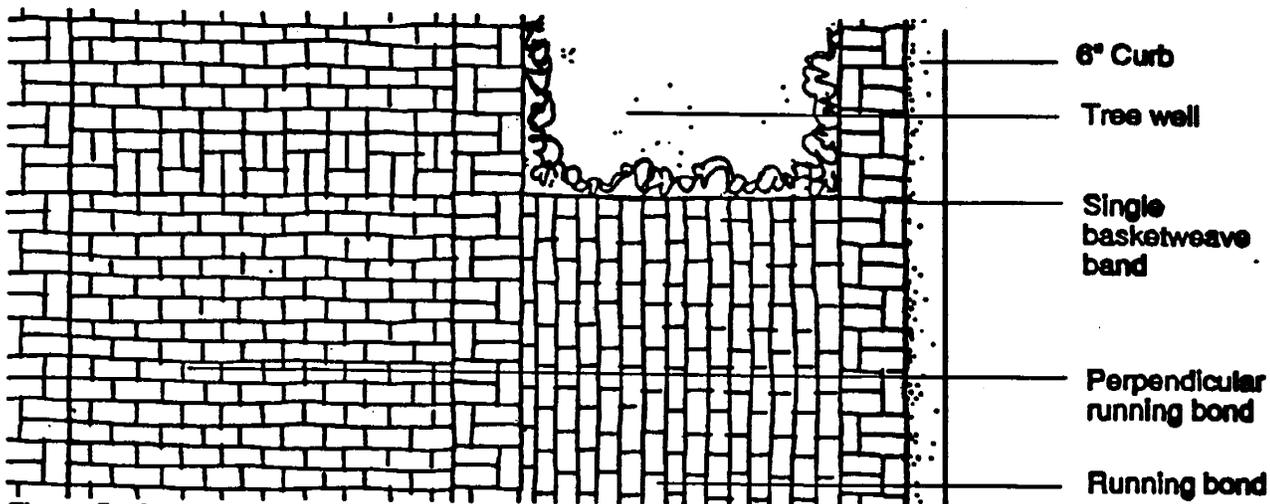


Figure 5-10: Jamieson Avenue Standard Sidewalk Paving



5.5 FURNITURE

5.5.1 SEATING

FORMAL SEATING - BENCHES: Benches shall be provided within Courthouse Square. They may also be provided if desired in the curb zone at the courthouse and Block F along the square.

5.5.2 STREET FIXTURES

TRASH RECEPTACLES: One trash receptacle shall be provided at all street corners. On Jamieson Avenue, one trash receptacle shall be provided for each 1600 linear feet of streetscape; the trash receptacles on the street corners may be included in the quantity to meet this requirement.

FENCES, GATES, COLUMNS AND WALLS: Fences, gates, columns and walls may only be used in association with residential uses in this precinct.

BOLLARDS: The use of decorative bollards is strongly encouraged in the Square.

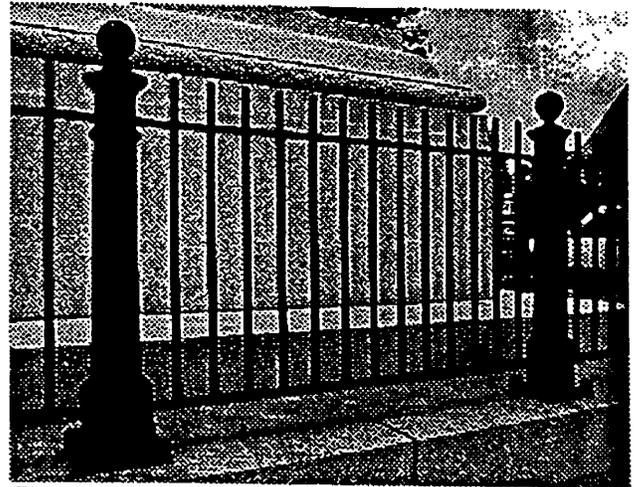


Figure 5-12: Civic Fence



5.6 LIGHTING

5.6.1 STREETLIGHTS

Streetlights in Jamieson Precinct are spaced according to the use of the area in the precinct. Along Jamieson Avenue which has retail uses, single luminaire streetlights will be spaced at sixty-four feet on center and paired across the street. The streetlights around Courthouse Square shall be paired single luminaire streetlights and will be treated in accordance with the requirements for lights at Major Lobby entrances with a base spacing of sixty-four feet on center. Streetlights with a double luminaire shall be located at the four interior corners of the square; refer to, Section 5.2.2 - Major Open Space. Streetlights on all other streets in Jamieson Precinct shall be single luminaire streetlights spaced at eighty feet on center and staggered across the street.

5.6.3 BUILDING LIGHTS

FACADE LIGHTING: Facade lighting of the courthouse is encouraged to highlight the civic architecture of the building. Facade lighting of all other buildings in Jamieson Precinct is prohibited.

5.7 PLANTING

5.7.1 STREET TREES

The street trees for Jamieson Precinct vary by location. Along Jamieson Avenue, the street tree shall be *Quercus phellos*, Willow Oak. These street trees shall be spaced at thirty-two feet on center. Around outside street edge of Courthouse Square, including the portion of Jamieson Avenue fronting onto the Square, the street tree shall be the Redmond Linden, *Tilia americana* 'Redmond'. These trees shall be spaced at twenty feet on center. No street trees shall be planted around the inside edge of the square. The street trees for the remainder of Jamieson Precinct shall be *Acer saccharum* 'Green Mountain', Green Mountain Sugar Maple; these shall be spaced at twenty feet on center.

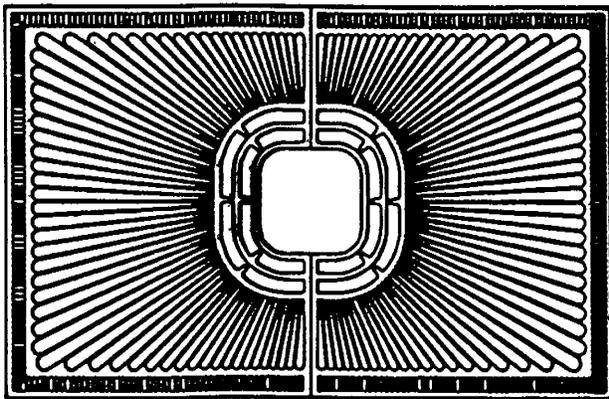


Figure 5-13: Centennial Series Tree Grate

Trees around the edge of Courthouse Square shall be installed in tree grates. The tree grate shall be a five foot by eight foot Centennial series tree grate by Ironsmith, Inc. Santa Ana, California.

All other street tree wells in Jamieson Precinct shall be flush with the pavement. These tree wells may be planted with groundcover plantings or seasonal flowers at the discretion of the parcel owner. The parcel owner shall be responsible for the maintenance of the plantings in the tree wells.

5.7.2 PLANTING BEDS AND PLANTERS

Planting beds and planters are not required along Jamieson Avenue. For the rest of Jamieson Precinct, planting beds and planters shall be provided for eighty percent of the net plantable facade area except in front of retail uses. Planters and planting beds are not required in front of retail uses, however they are encouraged. Planters and planting beds shall be a minimum of three feet wide and a maximum of five feet wide. Plantings along Courthouse Square shall be simple, lush and dignified.

5.7.3 FREESTANDING PLANTERS

The use of freestanding planters in the building zone along Jamieson Avenue in front of retail shops is encouraged. These planters shall not obstruct safe pedestrian movement and shall not extend more than two and a half feet from the building facade.



Figure 5-14: Freestanding Planters



5.8 SIGNS

5.8.2 INFORMATION SIGNS

RETAIL AND PROFESSIONAL SIGNS: Projecting signs are not permitted. Retail signs shall be a uniform design for each building.

PROJECT DIRECTORIES/KIOSKS: A project directory shall be provided at the Jamieson Avenue end of Courthouse Square.

5.10 ARCHITECTURAL PROJECTIONS

5.10.1 AWNINGS/CANOPIES:

Retail uses along Jamieson Avenue may have retractable awnings. Awnings when provided shall be uniform in size and shape, and an integral part of the building design.



Figure 5-15: Uniform Awnings



5.11 TRANSITIONAL AREAS

5.11.2 TRANSITIONS BETWEEN PRECINCTS

DULANY PRECINCT TRANSITION: Jamieson Precinct is subordinate to Dulany Precinct. At Jamieson Avenue and Dulany Streets, the sidewalk paving shall change from Jamieson Precinct to Dulany Precinct standards at the extension of the build-to-line parallel to Dulany Street. The street trees shall be continuous along their respective streets. The building zone plantings on Dulany Street shall end at the build-to-line parallel to Jamieson Avenue. See Figure 5-16.

At the Ballenger and Emerson Street intersection with Dulany Street the street trees shall be continuous along the respective streets. Standard sidewalk layout and paving will change from Jamieson to Dulany Precinct standards at the extension of the build-to line parallel to Dulany Street. See Figure 5-17. The building zone plantings shall extend to the end of the side building wall for the building that faces onto Dulany Street.

CARLYLE PRECINCT TRANSITION: Jamieson Precinct is subordinate to Carlyle Precinct. The standard sidewalk layout and paving shall change from Jamieson Precinct to Carlyle precinct standard paving at the line created by the extension of the build to line of Blocks C and E. See Figure 5-18.

EISENHOWER PRECINCT TRANSITION: Jamieson Precinct is subordinate to Eisenhower Precinct. The first two street trees along Elizabeth Lane shall be Bloodgood Plane Trees to match Eisenhower Precinct Street Trees. The first street tree shall align to the second row of street trees along Eisenhower Avenue. The standard sidewalk layout and paving will change at the extension of the build-to line parallel to Eisenhower Avenue. The planting requirements for Eisenhower Avenue shall wrap around the corner extending ten feet minimum, along Elizabeth Lane and ending at an appropriate architectural feature. See Figure 5-19.

HOLLAND PRECINCT TRANSITION: Jamieson Precinct is subordinate to Holland Precinct. The street trees shall remain the same along their respective streets. The standard sidewalk layout and paving shall change at the point of curvature of the curb radius on Jamieson Avenue. See Figure 5-20. Plantings from the building zone on Holland Lane shall extend around the corner a minimum of ten feet and end at an appropriate architectural feature.

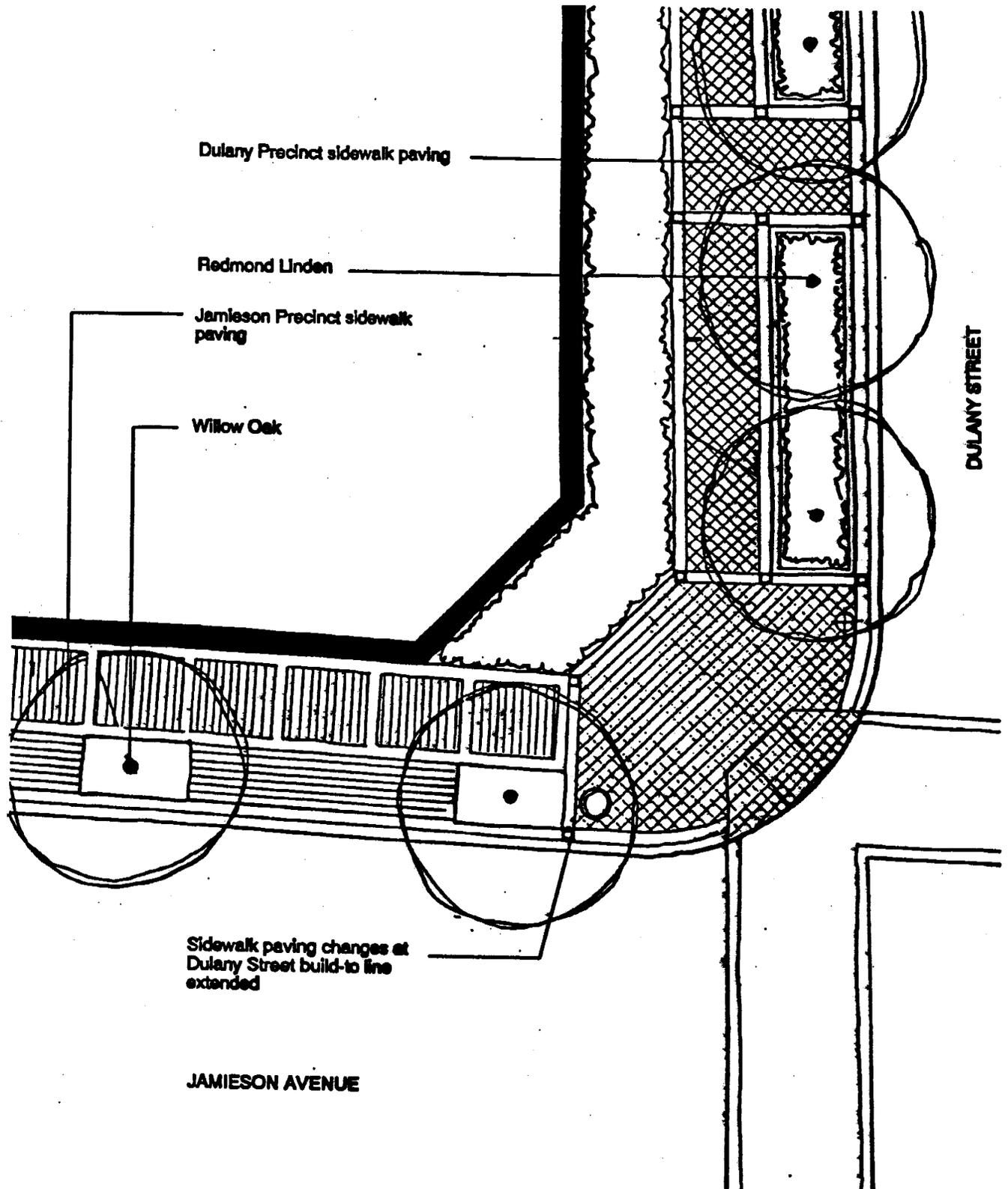


Figure 5-16: Dulany Precinct Transition at Jamieson Avenue

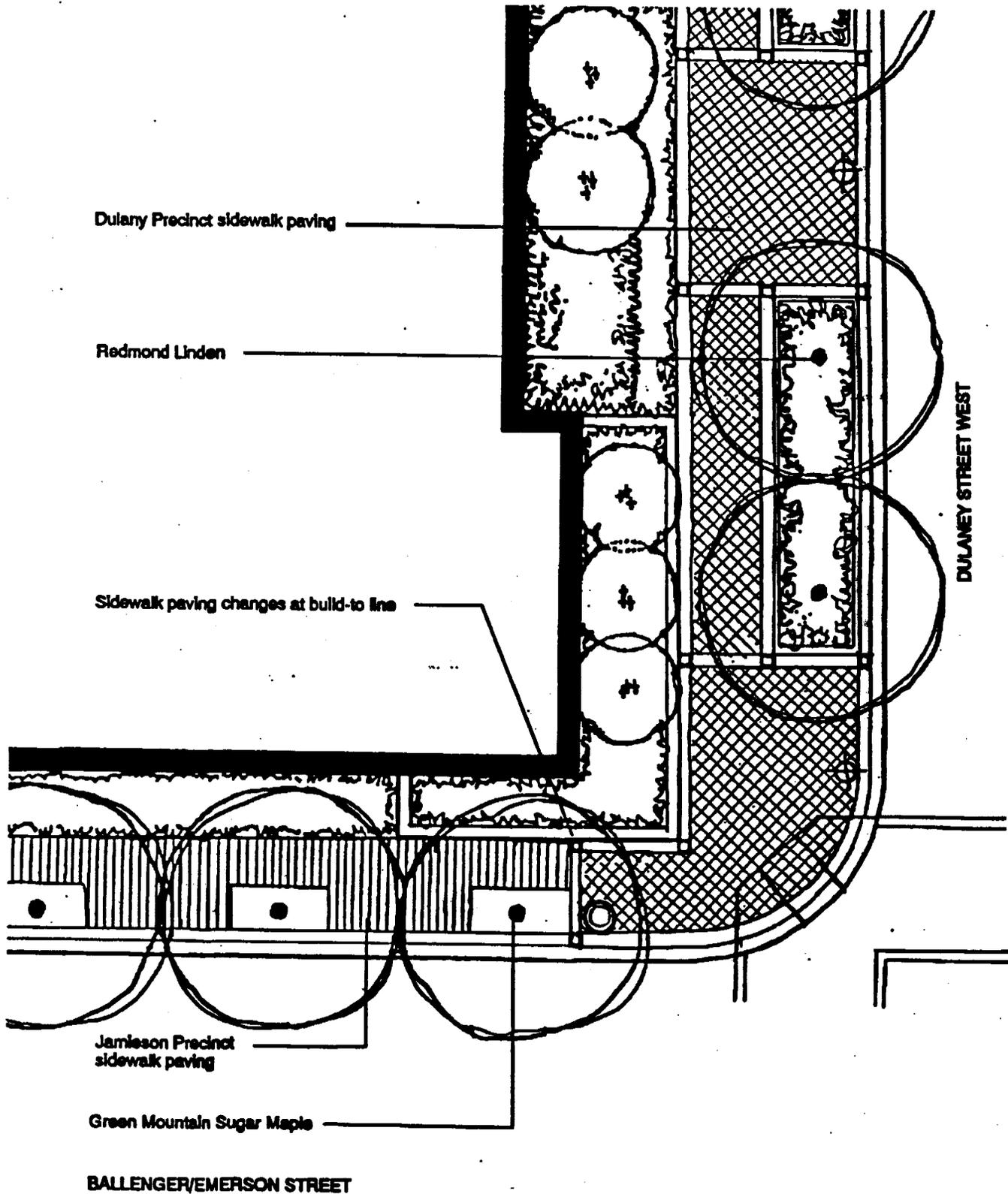


Figure 5-17: Dulany Precinct Transition at Ballenger and Emerson Streets

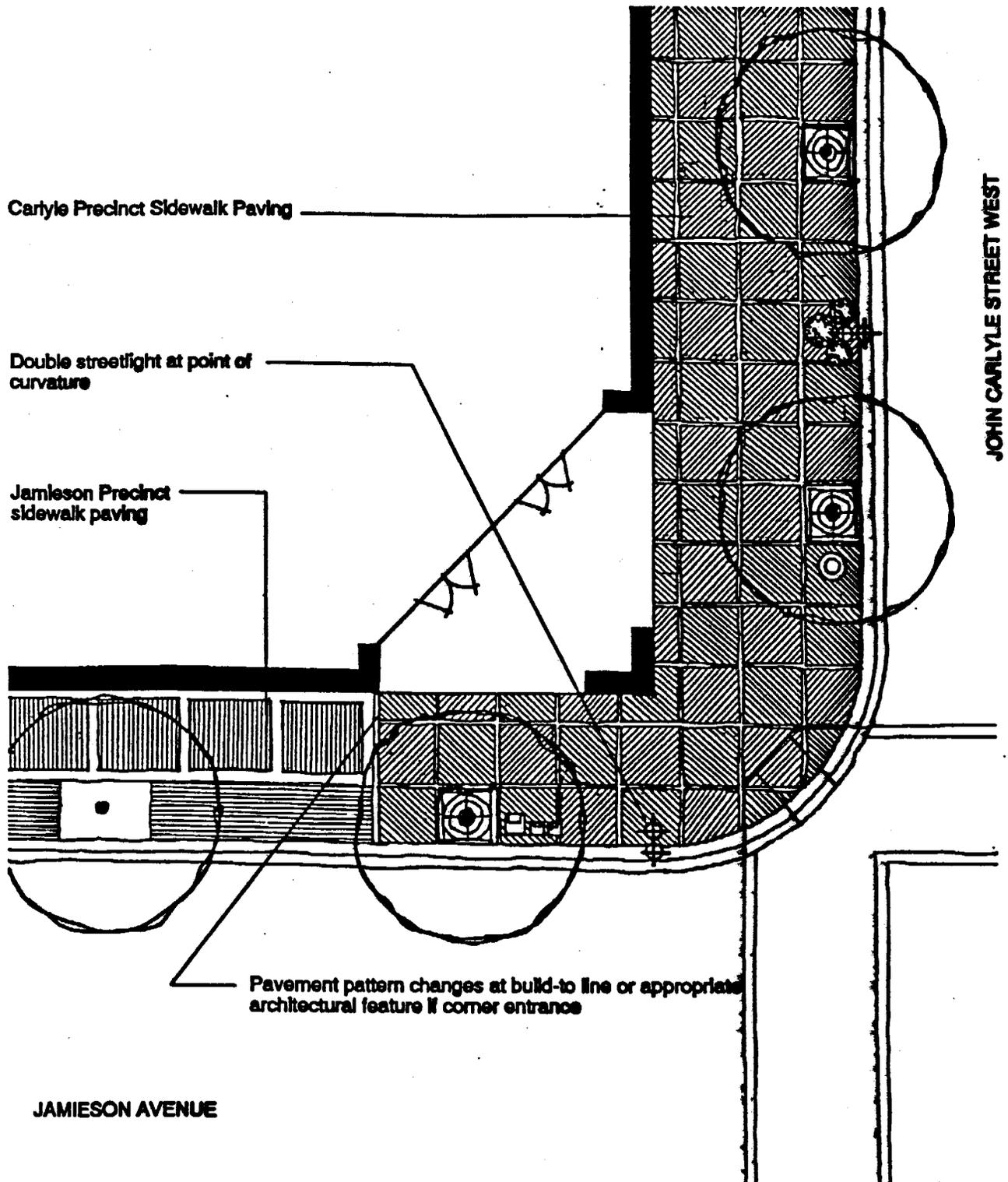


Figure 5-18: Carlyle Precinct Transition

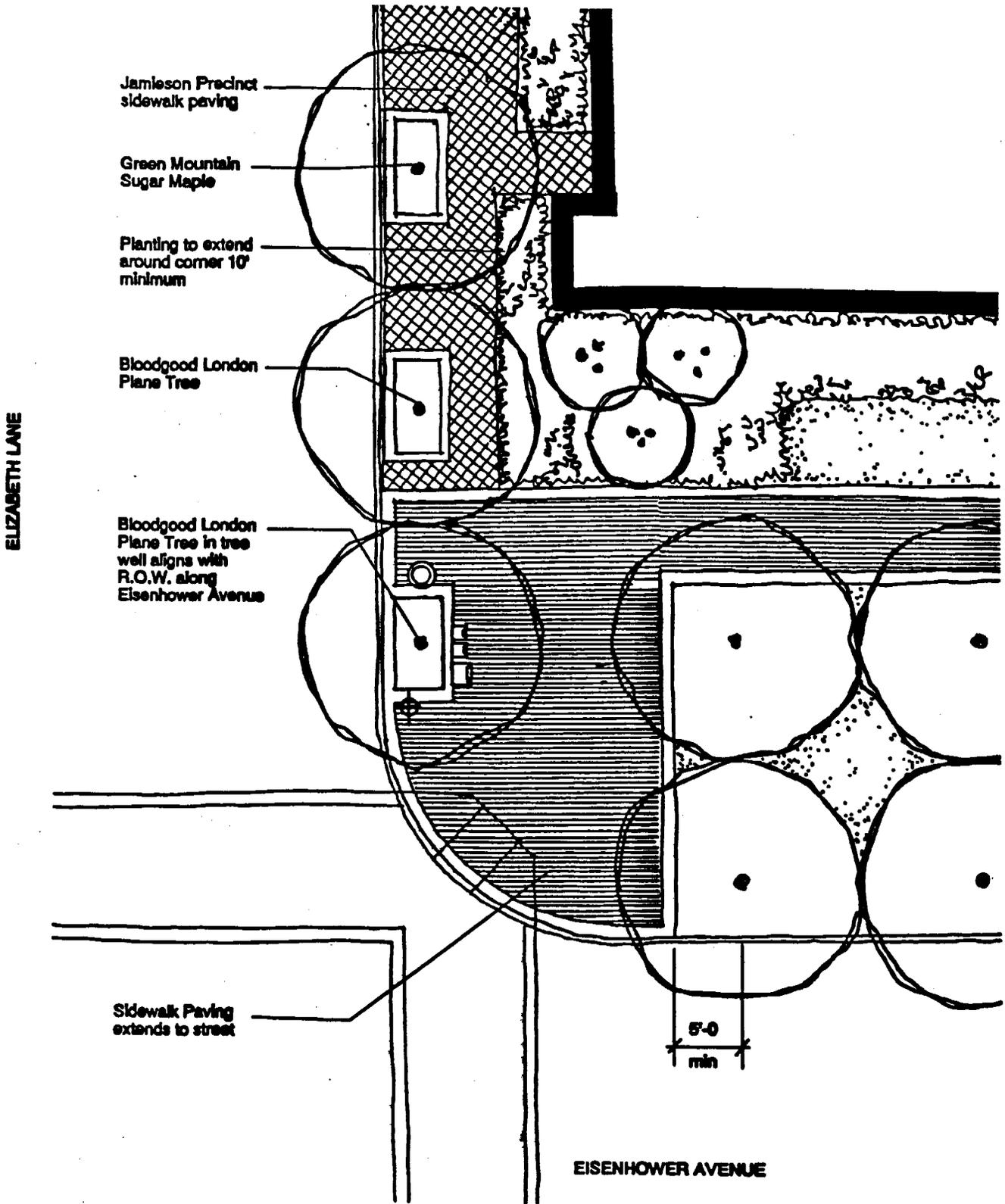


Figure 5-19: Eisenhower Precinct Transition

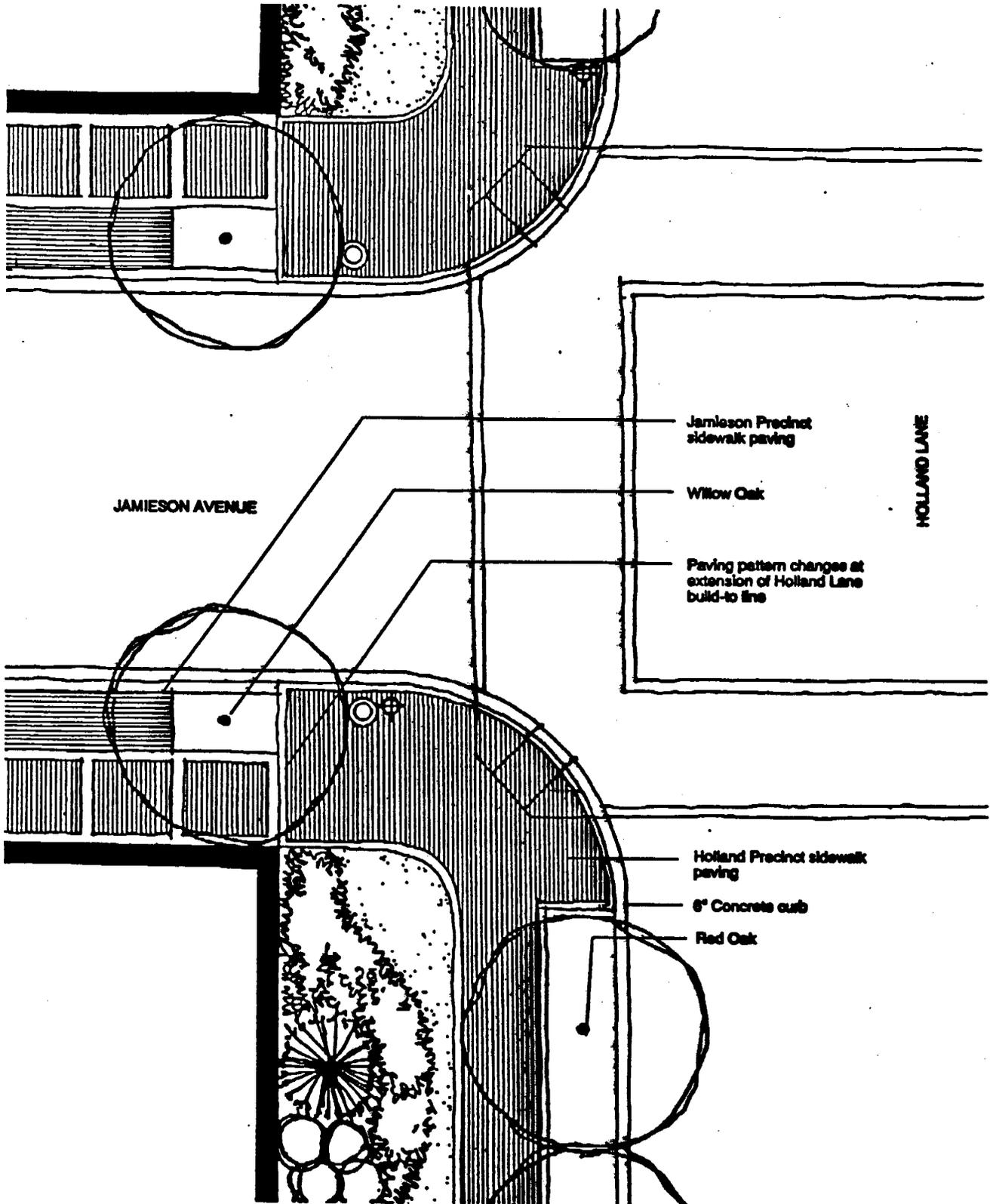


Figure 5-20: Holland Precinct Transition



5.11.3 TRANSITIONS TO AREAS ADJACENT TO CARLYLE

The general streetscape guidelines shall apply with the following addition.

WEST SIDE OF ELIZABETH LANE: The streetscape along the west side of Elizabeth Lane will be treated in a slightly different manner than the rest of Jamieson Precinct. A five and one half foot wide curb zone shall be provided; this curb zone shall be planted in lawn. Street trees will be planted in the lawn in the curb zone. A five foot wide walk shall be provided behind the curb zone. The walk shall be paved in a running bond pattern perpendicular to the street and shall have a twelve inch wide single basket weave band along each edge. The remaining three and a half foot area beyond the walk within the right-of-way shall be planted with an evergreen hedge.

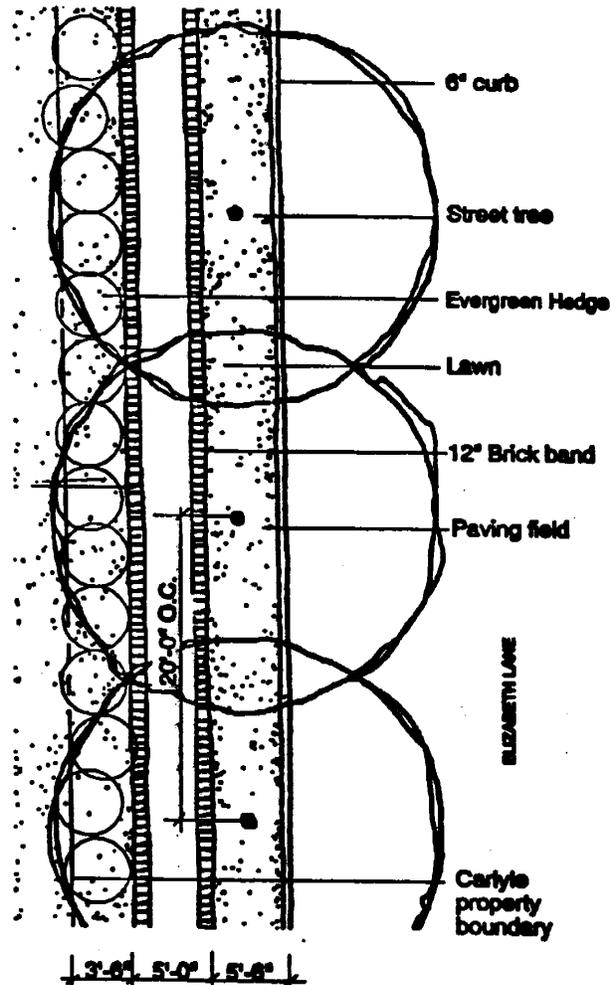


Figure 5-21: West Side of Elizabeth Lane

6 EISENHOWER PRECINCT

Additional information and special requirements for the streetscape for Eisenhower Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

<p>6.1 Design Concept and Character 6-3</p> <p> 6.1.1 Context 6-3</p> <p> 6.1.2 Use 6-4</p> <p> 6.1.3 Imagery 6-4</p> <p>6.2 Major Open Space 6-5</p> <p>6.3 Streetscape Design 6-6</p> <p> 6.3.1 Typical Layout and Dimensions 6-6</p> <p> 6.3.3 Vehicular Access Zones ... 6-7</p> <p> • Drop Offs/Lay-Bys</p> <p> 6.3.4 Intersections 6-7</p> <p> • Sidewalk Paving</p> <p>6.4 Hardscape 6-8</p> <p> 6.4.1 Paving 6-8</p> <p> • Standard Sidewalk Paving</p> <p> • Pavement Edges</p> <p> • Special Paving</p> <p>6.5 Street Furniture 6-9</p> <p> 6.5.1 Seating 6-9</p> <p> 6.5.2 Street Fixtures 6-9</p> <p> • Trash Receptacles</p> <p> • Bike Racks</p> <p> • Clocks, Art and Special Features</p> <p> • Bollards</p> <p>6.6 Lighting 6-10</p> <p> 6.6.1 Streetlights 6-10</p> <p> 6.6.2 Specialty Landscape Lighting 6-10</p> <p> • Miniature Lighting</p> <p> 6.6.3 Building Lights 6-10</p> <p> • Facade Lighting</p> <p>6.7 Planting 6-11</p> <p> 6.7.1 Street Trees 6-11</p>	<p>6.7.2 Planting Beds and Planters 6-11</p> <p>6.7.3 Free Standing Planters 6-11</p> <p>6.10 Architectural Projections 6-12</p> <p> 6.10.1 Awnings/Canopies 6-12</p> <p> 6.10.2 Porte Cocheres 6-12</p> <p>6.11 Transitional Areas 6-13</p> <p> 6.11.2 Transitions between Precincts 6-13</p> <p> • Jamieson Precinct Transition</p> <p> • Dulany Precinct Transition</p> <p> • Ballenger Precinct Transition</p> <p> • Holland Precinct Transition</p> <p> 6.11.3 Transitions to Areas Adjacent to Carlyle 6-18</p> <p> • South Side - Eisenhower Avenue</p>
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6 EISENHOWER PRECINCT

Eisenhower Avenue is a gateway to Old Town from the west. As the development of the Eisenhower Valley increases, the importance of Carlyle at the eastern end of Eisenhower Avenue will be greater. The design concept for Eisenhower Precinct is a clean, simple strong boulevard lined with trees and lawn on each side. The terminus of Eisenhower Avenue is at the Rotary within Carlyle. The boulevard treatment will set apart Carlyle from the rest of Eisenhower Avenue to the west and focus views to the Rotary.

The Eisenhower Precinct forms the southern edge of Carlyle; it extends from Elizabeth Lane east to John Carlyle Street.

6.1 DESIGN CONCEPT AND CHARACTER

The design concept for Eisenhower Precinct is for the boulevard to impart the character of a corporate office address.

6.1.1 CONTEXT

SITE PLAN: Eisenhower Precinct is set amidst nine story office buildings which are set back thirty-nine feet from the right-of-way. The large buildings with the generous setbacks create a grand scale in conjunction with the four lane roadway with median. The scale of the street relates more to the buildings and automobiles than the pedestrian. Despite the scale of the street, the urban feel of Carlyle is maintained by the consistent building line.

Part of the precinct is the southern edge of Carlyle and adjoins existing office and warehouse buildings along the south side of Eisenhower Avenue. This will be a place of transition in the streetscape. Eisenhower Precinct also becomes an entrance to Dulany Precinct and secondarily to Jamieson and Ballenger Precincts.

CIRCULATION: Eisenhower Avenue as a gateway to Old Town, will see a heavy amount of through automobile traffic. It will be used by many commuters on their way to and from work daily.

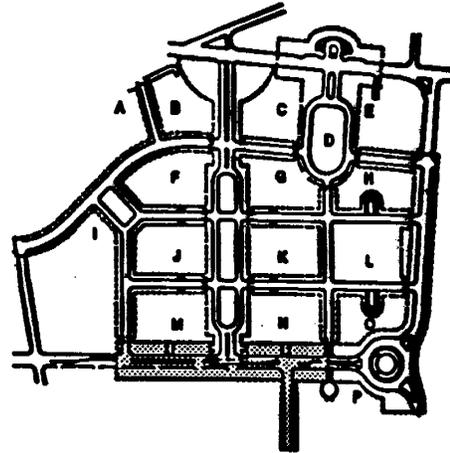


Figure 6-1: Eisenhower Precinct

Much of the activity from the automobiles and buses in the street to the pedestrians will be the movement of people to other destinations within Carlyle and elsewhere such as the Eisenhower Avenue Metro Station, which is just west of Carlyle on Eisenhower Avenue.

Part of the City of Alexandria Bike Trail System runs along Eisenhower Avenue. This bike trail will be an alternative for commuters to use to travel to work and connect Carlyle to the City's recreational trail system. Many other cyclists will pass through Carlyle on this trail.

OPEN SPACE: There is no major open space within Eisenhower Precinct. The precinct does, however, have a strong focus on the Rotary at Holland Lane due to the axial relationship of Eisenhower Avenue to the Rotary. The broad greensward of the streetscape will also provide a balance to the wide paved street.

To a lesser degree, Dulany Gardens has an influence on Eisenhower Precinct as well. Dulany Street marks that most prominent gateway to the interior of Carlyle on the southern side. The Gardens being close by, push Dulany Precinct out onto Eisenhower Avenue at Dulany Street.



6.1.2 USE

The buildings along Eisenhower Avenue will be predominantly office use with the major first floor use being office as well. The buildings may be single corporate headquarters or occupied by single major office tenants. The major entrances to these buildings will have the strongest relationship to the streetscape due to the scale of the street and the buildings. The office uses are semi-public to private uses so much of the activity will be from people who work and are visiting the offices.

6.1.3 IMAGERY

The classic boulevard image is that of a broad street with trees in lawn. The trees enhance the line and focus of the boulevard. Preferably the large office buildings should project corporate identities. The images may be defined through the use of large scale architectural detailings and unified bold massive planting along the buildings.



Figure 6-2: Queens Road, Charlotte, North Carolina

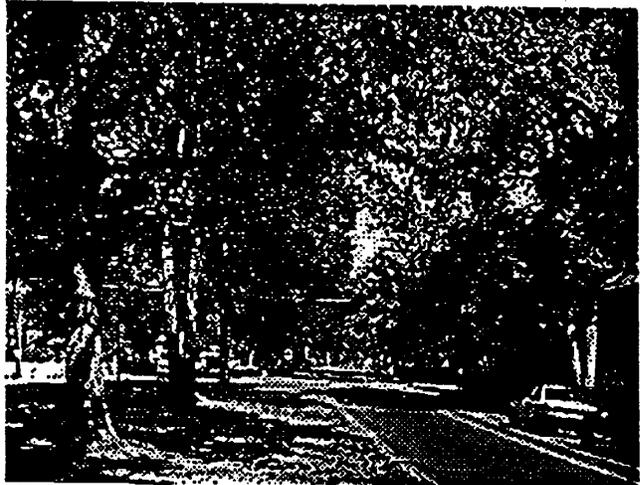


Figure 6-3: Commonwealth Avenue, Alexandria, Virginia



Figure 6-4: Large Scale Architectural Detailing



6.2 MAJOR OPEN SPACE

There is no major open space within Eisenhower Precinct; however, the Rotary in Holland Precinct is the focus of the Eisenhower Avenue axis. Refer to Section 7.2 for more information on the Rotary.



6.3 STREETScape

6.3.1 TYPICAL LAYOUT AND DIMENSIONS

Eisenhower Precinct's streetscape is designed to strengthen the boulevard feeling of Eisenhower Avenue. The curb zone is thirty feet wide and contains a double row of trees set in lawn. The first row of trees shall be five feet from the face of curb; the second row shall be twenty-five feet from the face of curb. The curb zone shall be kept generally free and clear of furniture to enhance the boulevard image. The pedestrian zone shall consist of an eight foot walk located immediately behind the curb

zone. The building zone shall be the remaining area between the pedestrian zone and the building facade.

Along Hooff's Run Drive, the curb zone shall be five feet wide with street trees planted centered in lawn. The pedestrian zone shall consist of a four foot wide sidewalk with eight inch bands on each side. The building zone shall consist of the remainder of the right-of-way and the two foot sidewalk easement.

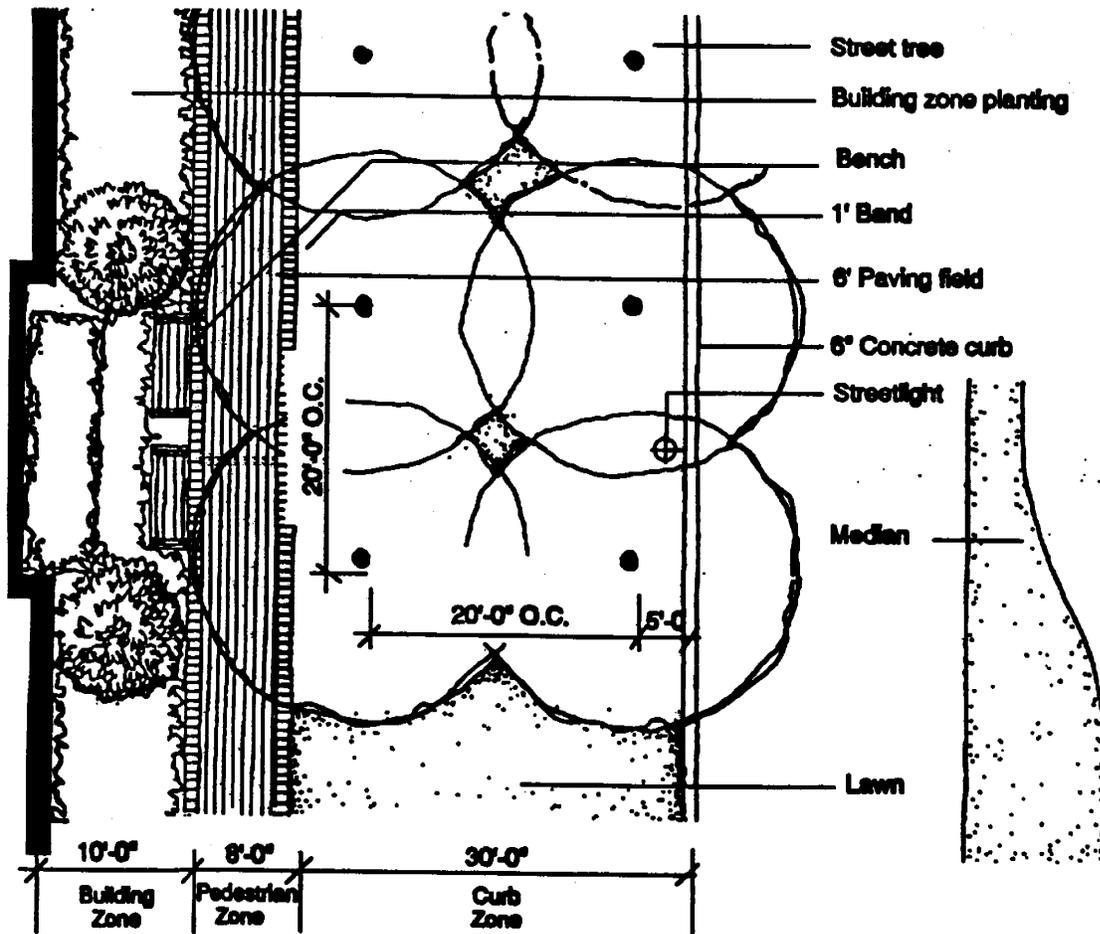


Figure 6-5: Eisenhower Precinct - Typical Streetscape Layout

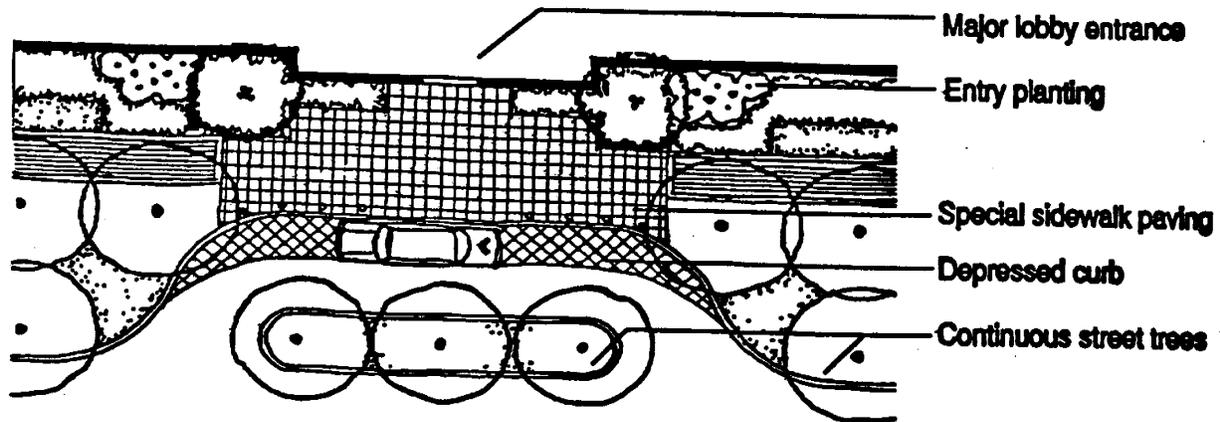


Figure 6-6: Eisenhower Avenue Dropoff

6.3.3 VEHICULAR ACCESS ZONES

DROPOFF/LAY-BYS: Dropoffs shall not substantially interrupt the first row of street trees along Eisenhower Avenue. In order to accomplish this an island shall be provided between the dropoff and the street; this island shall contain a minimum of three street trees spaced at twenty feet on center, aligned with the first row of street trees along Eisenhower Avenue. The spacing of these street trees in relation to the balance of the street trees along Eisenhower Avenue may need to be adjusted, however, these trees shall be arranged symmetrically about the building entrance centerline.

The spacing of streetlights may also need to be adjusted in order to best fit the specific design of the dropoff.

6.3.4 INTERSECTIONS

SIDEWALK PAVING: At all intersections, sidewalk paving shall extend to the curb along Eisenhower Avenue. Since all intersections in the Eisenhower Precinct are transitional areas to other precincts, refer to Section 6.11.2 - Transitions to Other Precincts, for specific requirements at each intersection.



6.4 HARDSCAPE

6.4.1 PAVING

STANDARD SIDEWALK PAVING: The standard sidewalk paving for Eisenhower Precinct shall be the King William Range Brick paver laid in a running bond pattern parallel to the street. A twelve inch brick band shall be provided along each edge of the sidewalk and along the curb where paving abuts the curb. The banding shall be laid in a running and stack bond.

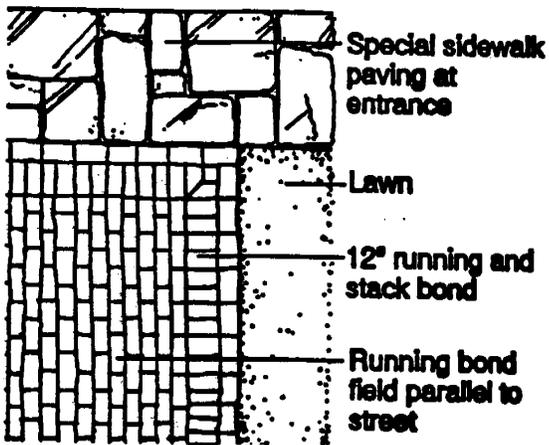


Figure 6-7: Eisenhower Precinct - Typical Paving Pattern

Along Hooff's Run Drive the paving shall be similar except the sidewalks shall have an eight inch rowlock band on each side.

PAVEMENT EDGES: All sidewalk and planter bed edges shall be flush with grade.

SPECIAL SIDEWALK PAVING: Special sidewalk paving when provided shall respond to the scale of the boulevard and the emphasis of major lobby entrances. Special paving may be integrated with the dropoffs and lay-bys as well.

6.5 FURNITURE

6.5.1 SEATING

Except as otherwise noted, seating may be provided in the building zone only.

6.5.2 STREET FIXTURES

TRASH RECEPTACLES: One trash receptacle shall be provided at each corner of all intersections.

BIKE RACKS: If bike racks are needed; they shall be located within the building zone on standard sidewalk paving.

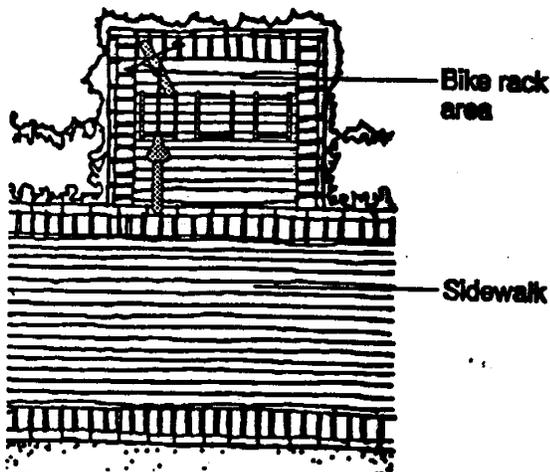


Figure 6-8: Bike Rack Area

CLOCKS, ART AND OTHER SPECIAL FEATURES: Clocks, art and other special features are permitted in the building zone only.

BOLLARDS: The use of bollards in lay-bys and dropoffs is encouraged.



6.6 LIGHTING

6.6.1 STREETLIGHTS

Single luminaire streetlights as specified in the General Streetscape Guidelines shall be provided along Eisenhower Avenue. They shall be spaced at eighty feet on center and paired across the street at Blocks O and P. Streetlights shall also be spaced at eighty feet on center along Hooff's Run Drive.

6.6.2 SPECIALTY LANDSCAPE LIGHTING

Along Eisenhower Avenue additional low level lighting may be necessary along sidewalks in order to meet the minimum lighting standards of the City of Alexandria.

MINIATURE LIGHTING: Miniature lights are prohibited in Eisenhower Precinct.

6.6.3 BUILDING LIGHTS

The general streetscape guidelines shall apply with the following addition.

FACADE LIGHTING: Lighting of facades at major entrances is encouraged in order to enhance the importance of entrances on Eisenhower Avenue.



6.7 PLANTING

6.7.1 STREET TREES

The street trees along Eisenhower Avenue shall be the Bloodgood Plane Tree, *Plantanus x acerfolia* 'Bloodgood'. Along Hooff's Run Drive the street trees shall be *Tilia cordata* 'Greenspire'; the Greenspire Linden. The street trees shall be planted twenty feet on center in lawn.

6.7.2 PLANTING BEDS AND PLANTERS

All planting shall be installed in at-grade planting beds within the building zone. The parcel owner shall provide continuous planting along 100% of the net plantable building facade. Plantings shall be a minimum of five feet wide and may extend to fill the building zone.

Planting design shall provide large masses to reinforce the grand boulevard scale of Eisenhower Avenue. Loose, unstructured, naturalistic planting design and plants such as unclipped Abella, Forsythia or River Birch are not acceptable. Acceptable plants include Yews, Azaleas and Cherries.

6.7.3 FREESTANDING PLANTERS

Freestanding planters are prohibited in Eisenhower Precinct.



**6.10 ARCHITECTURAL
PROJECTIONS**

6.10.1 AWNINGS/CANOPIES

Awnings and canopies are not permitted in Eisenhower Precinct.

6.10.2 PORTE COCHERES

Porte cocheres are permitted only in conjunction with dropoffs/lay-bys in Eisenhower Precinct.



6.11 TRANSITIONAL AREAS

6.11.2 TRANSITIONS BETWEEN PRECINCTS

JAMIESON PRECINCT TRANSITION: Eisenhower Precinct is dominant over Jamieson Precinct. The first two street trees on each side of Elizabeth Lane shall be Bloodgood Plane Trees to match the Eisenhower Precinct street trees. The first street tree on Elizabeth Lane shall align with the second row of street trees along Eisenhower Avenue. The standard sidewalk layout and paving shall change from Eisenhower Precinct standards to Ballenger Precinct standard paving at the line created by the extension of the build-to line parallel to Eisenhower Avenue. The planting requirements for the building zone for Eisenhower Precinct shall wrap around the corner extending ten feet minimum along Elizabeth Lane and ending at an appropriate architectural feature. See Figure 6-9.

DULANY PRECINCT TRANSITION: Eisenhower Precinct is subordinate to Dulany Precinct. The two corners of Dulany Street and Eisenhower Avenue shall be treated symmetrically. See Figure 6-10.

The area in front of the portal elements of Blocks M and N shall be designed to create small plazas in front of the building. The plazas shall be separated from the street edge and relate to the minor entrances at the portal elements.

The standard sidewalk layout and paving for Dulany Precinct shall extend to Eisenhower Avenue and the courtyard shall be paved with Dulany Precinct paving. The sidewalk paving shall change at the edge of the plaza to Eisenhower Precinct standard sidewalk paving.

The Dulany Precinct street trees shall extend to Eisenhower Avenue. There shall be three trees arranged in a quarter circle at each corner, with the one closest to Eisenhower Avenue aligning with the first row of trees along Eisenhower Avenue.

The planters in the building zone for Dulany Precinct shall extend around the corner to Eisenhower Avenue a minimum of ten feet ending at an appropriate architectural feature. These planters

shall be coordinated with the plaza design as well. Each plaza shall have at least one bench and one directory/kiosk within it.

BALLENGER PRECINCT TRANSITION: Eisenhower Precinct is dominant over Ballenger Precinct. The first two street trees on each side of John Carlyle Street shall be Bloodgood Plane trees to match the Eisenhower Avenue street trees. The first tree on John Carlyle Street shall align with the second row of street trees along Eisenhower Avenue. The standard sidewalk paving shall change from Eisenhower Precinct standard paving to Ballenger Precinct standard at the line created by the extension of the build-to line parallel to Eisenhower Avenue. The planting requirements for the building zone for Eisenhower Precinct shall wrap around the corner extending ten feet minimum along Elizabeth Lane and ending at an appropriate architectural feature. Eisenhower Precinct and Holland Precinct are similar in design; neither shall be dominant. Each precinct's design shall extend to the curb on either side of John Carlyle Street. The location of the street trees along John-Carlyle Street shall be coordinated to begin across from each other, and any other element shall be coordinated across John Carlyle Street. See Figure 6-11.

HOLLAND PRECINCT TRANSITION: Holland Precinct and Eisenhower Precinct streetscape design is similar. The transition between the two precincts at John Carlyle Street shall be made so that neither is dominant, and the experience of Eisenhower Avenue is continuous. See Figure 6-12. At Block P, the first two street trees along John Carlyle Street shall be Bloodgood Plane trees to match those along Holland; the first tree shall align to the second row of street trees along Eisenhower Avenue.



ELIZABETH LANE

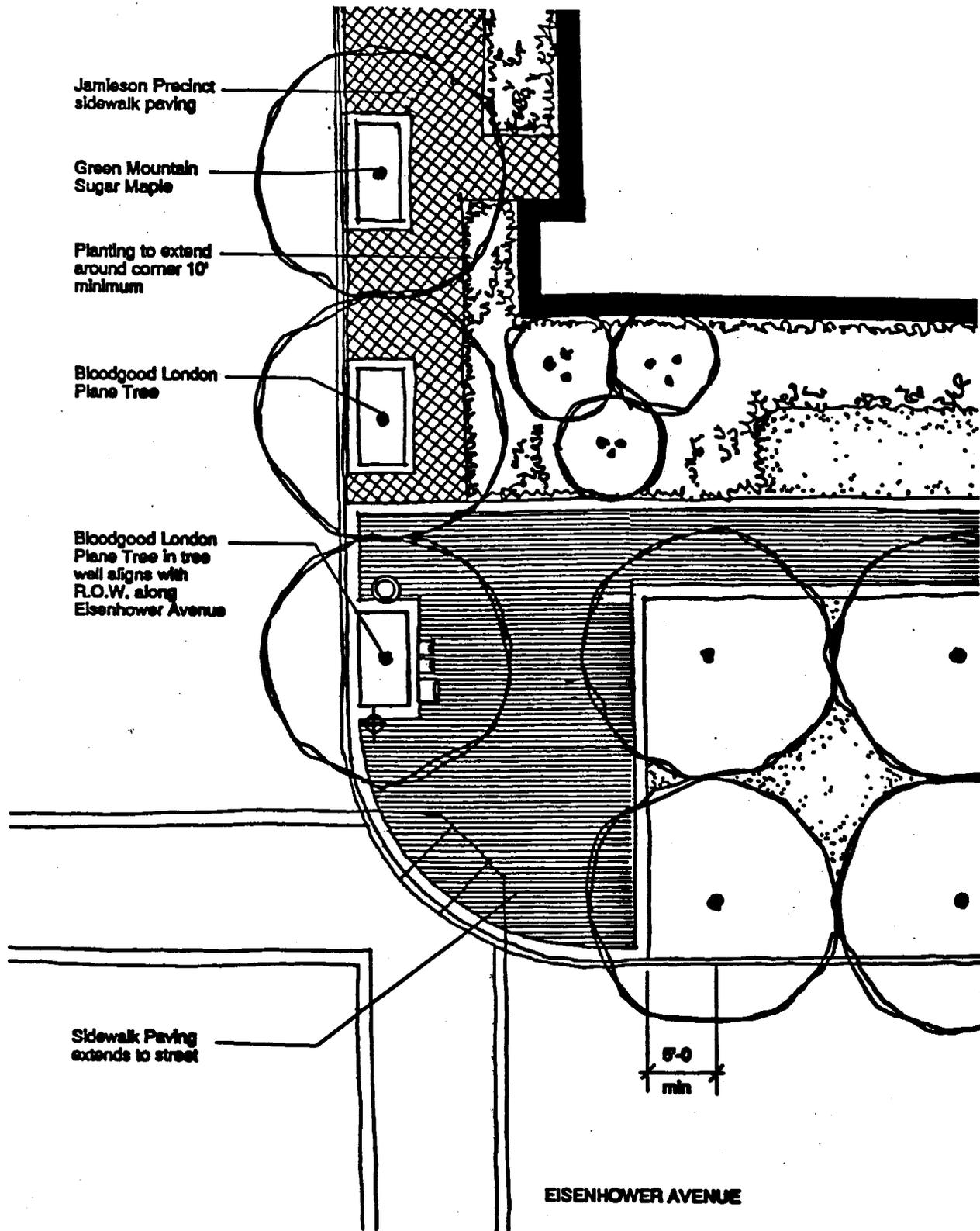


Figure 6-9: Jamieson Precinct Transition

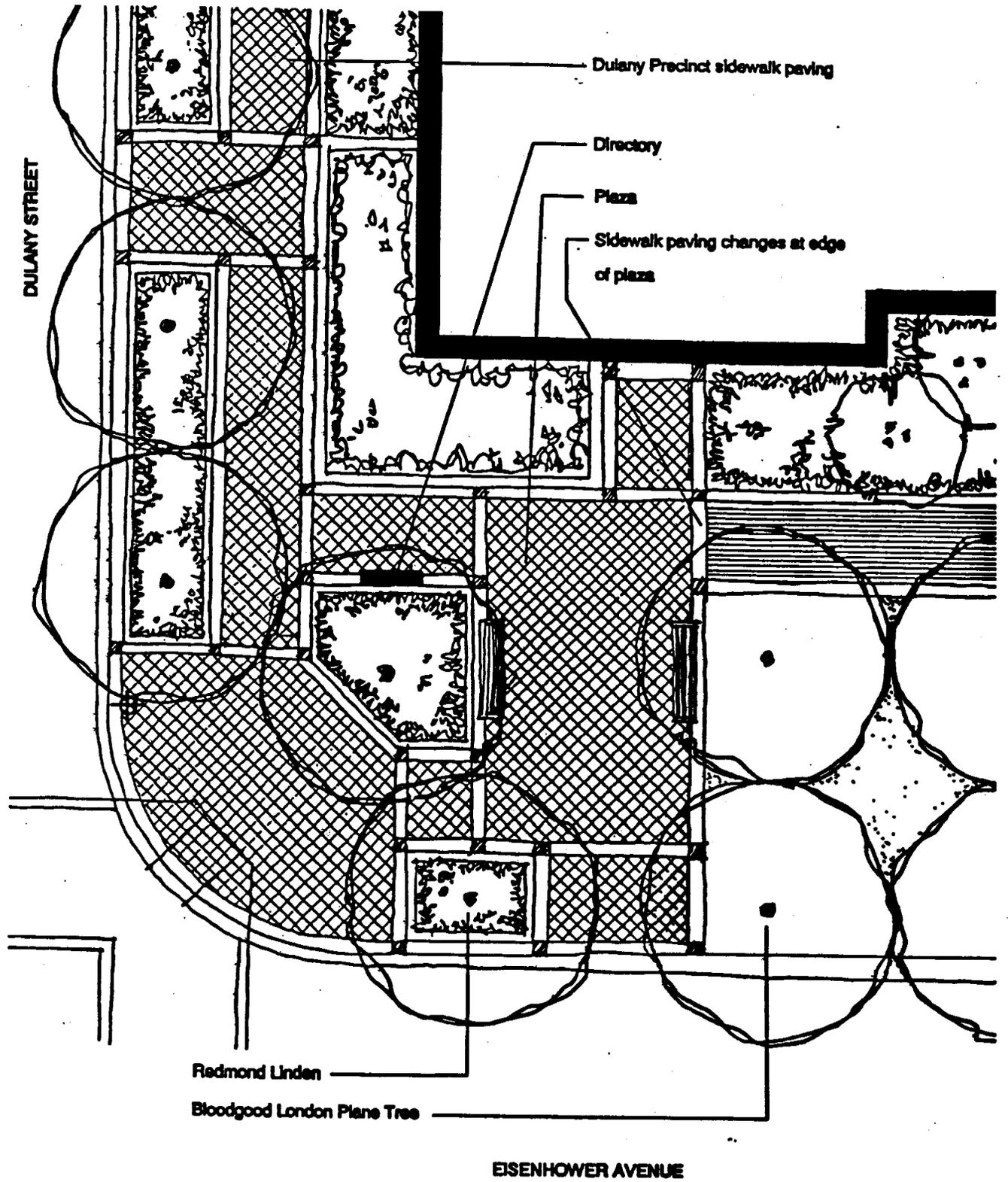


Figure 6-10: Dulany Precinct Transition

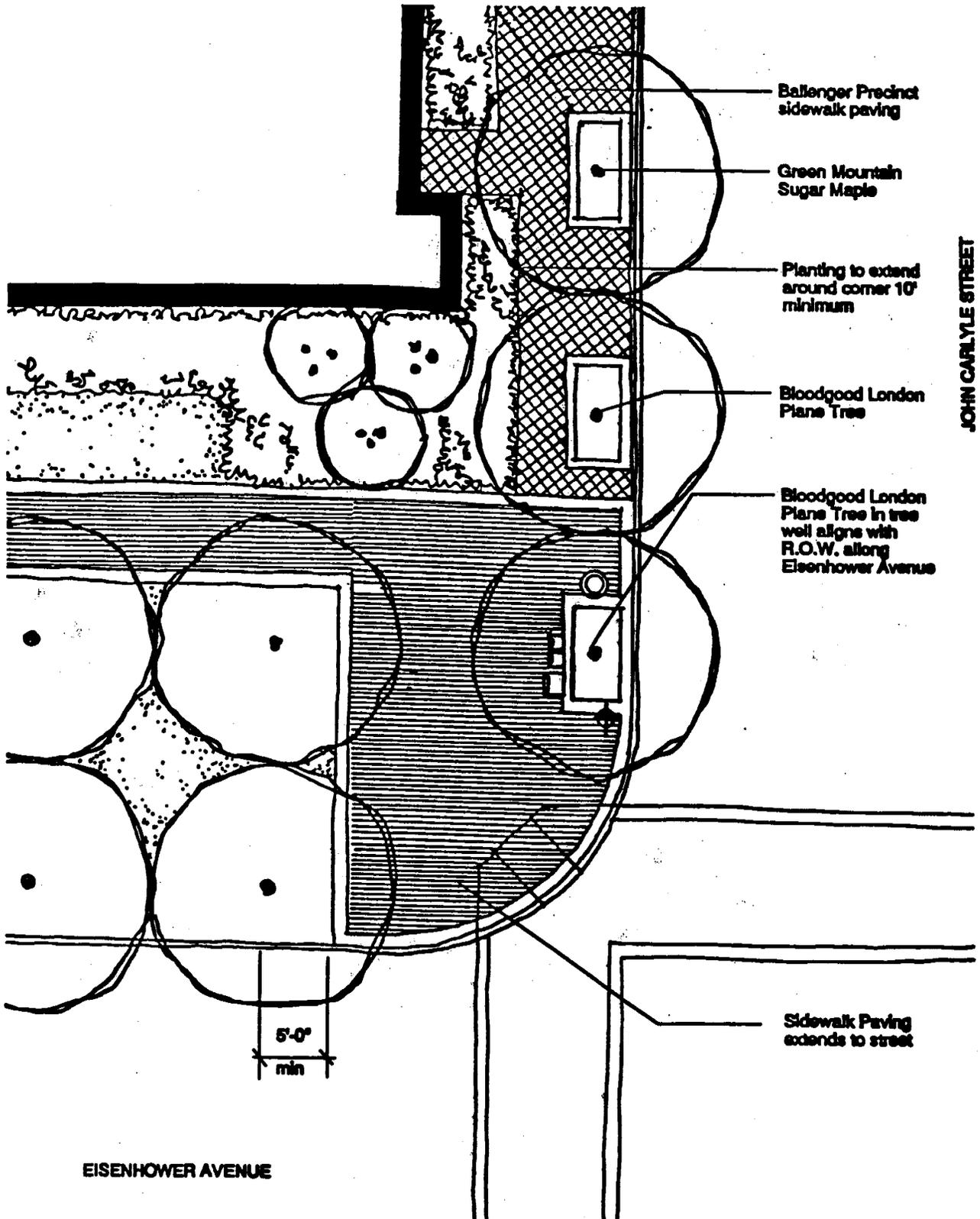
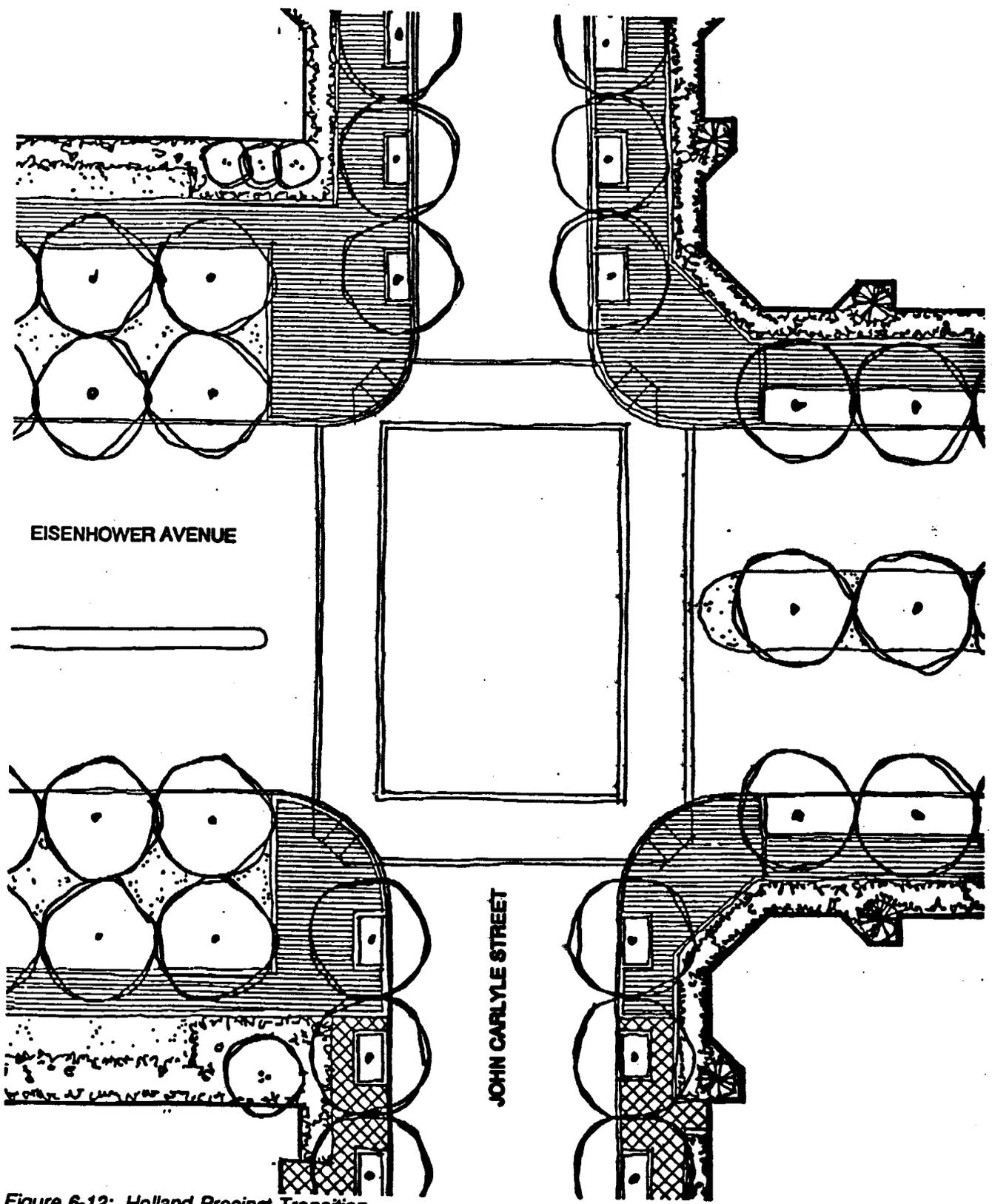


Figure 6-11: Ballenger Precinct Transition



EISENHOWER AVENUE

JOHN CARLYLE STREET

Figure 6-12: Holland Precinct Transition

7 HOLLAND PRECINCT

Additional information and special requirements for the streetscape for Holland Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

7.1 Design Concept and Character	7-3	7.8 Signs	7-13
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7. HOLLAND PRECINCT

A parkway is the design concept for Holland Precinct. The visual prominence of the nine acre public park adjoining this side of Carlyle will give this precinct the feel of being within a park. This park setting plays the strongest role in reinforcing the fifth principle of the development plan: to have the streets be a linear park. The goal for Holland Precinct is to enhance and reinforce the strong relationship between the park, the rotary, and a residential neighborhood.

Cooper Robertson and Partners summed up the design intent for Holland Precinct in the Design Report for Carlyle when they stated that Holland Lane is:

...envisioned as a tree lined boulevard with ... brick sidewalks and landscaped areas with... London Plane or similar type trees. The residential buildings which front this street and park will be configured so that a series of landscaped courtyards will face the park as well. These buildings will be developed to varying heights (four-to-six stories up to fifteen story towers), and street wall configurations to accentuate this parkway-like setting of the residential neighborhood.

Holland Precinct is located at the eastern edge of Carlyle. It is intended that the strong visual relationship with the park will be reinforced with informal naturalistic planting interweaving the park and the urban edge of Carlyle.

7.1 DESIGN CONCEPT AND CHARACTER

7.1.1 CONTEXT

Because Holland Precinct is on the eastern edge of Carlyle, this edge position makes it a strongly identifiable place due to the site plan and use of the precinct.

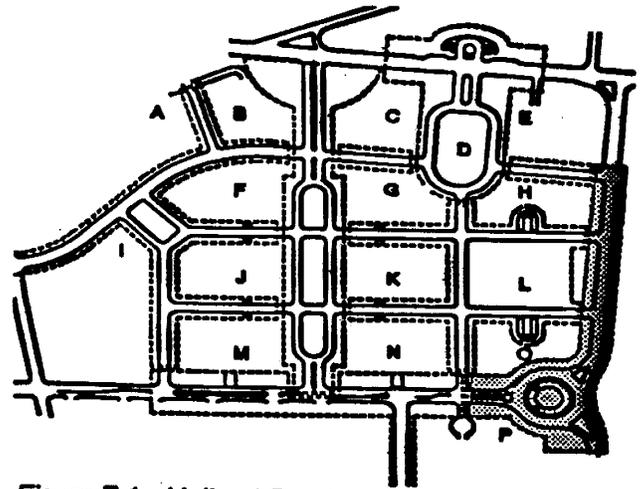


Figure 7-1: Holland Precinct

SITE PLAN: The combination of four, seven and fifteen story residential buildings along Holland Lane sharply define the western edge of the precinct. These buildings will be oriented to the park and will have a strong visual connection to the park and Old Town beyond over the tops of the trees. The mass of the buildings will be broken with street level and second story garden courts that will address Holland Lane and the sidewalk at street level. The primary entrances to these buildings will be on the opposite sides of the buildings facing onto Ballenger Mews; entrances on Holland Lane will be secondary. The streetscape ribbon of sidewalk and lawn along Holland Lane will not be interrupted by parking and service entrances, enhancing the park effect.

CIRCULATION: Because Holland Lane connects the Eisenhower Valley to Old Town, the amount of vehicular traffic on Holland Lane will be substantially more than on the interior streets of Carlyle. Holland Lane intersects Eisenhower Avenue at the Rotary in the southeast corner of Carlyle. The nature of the traffic on Holland Lane will be more through than local even though it provides access to Carlyle at the three east-west streets. The intersections of these three streets with Holland Lane provide strong points for pedestrian crossings to the park. The pedestrian traffic along Holland Lane will probably be minimal as most people will be going to the park or their residence. The bike trail along Holland Lane will add through bike traffic and provide easy access for Carlyle residents to the Alexandria bike trails system.



OPEN SPACES: There are two open spaces in the Holland Precinct; they are Alexandria African American Park and the Rotary.

Alexandria African American Park, which runs the length of the eastern edge of Carlyle, has large existing trees and other vegetation. Most of this will be preserved in the design for the park. These existing trees more than anything else establish the parkway image for Holland Precinct. The existing grade at Holland Lane is high and it quickly falls to a lower level at Hooff's Run.

The Rotary at the intersection of Holland Lane and Eisenhower Avenue will be a place many people will associate with their image of Carlyle because they will drive around it. The rotary is a strong geometric form and this form is reinforced by the buildings surrounding the west side of it. The Rotary is like many of the circles in Washington which are a focus for a neighborhood.

7.1.2 USE

In Holland Precinct, the major first floor use and the predominant precinct use are residential. Passive recreational use of the park is the next most important use in the precinct. The park provides a destination for pedestrians in the streetscape as well as influencing the character of Holland Lane. The park is a public use while the residential buildings are private uses. The street is the interface which provides the transition between the two.

Activity in Holland Precinct streetscape will be fairly constant through the day, with automobile traffic peaking at the morning and evening rush hours. Weekends will see a higher concentration of people strolling to the park and using the bike trails along Holland Lane especially during daylight hours.

7.1.3 IMAGERY

The image of a parkway is a pastoral one; that of a greensward containing large trees and lawn. The passive park is a place to stroll and enjoy picturesque views.



Figure 7-2: Trees and Lawn



Figure 7-3: Connecticut Avenue

The adjoining residential buildings set in this parkway will be very much like Connecticut Avenue in Washington or Central Park in New York.



7.2 MAJOR OPEN SPACES

The major open spaces will contribute substantially to the parkway image of Holland Precinct. Every effort should be made to orient buildings and integrate the built edge of Carlyle to these spaces.

7.2.1 ROTARY

The Rotary at the intersection of Holland Lane and Eisenhower Avenue is the terminus to the long axis of the Eisenhower "boulevard." The strong geometric form of the circle defines the space and its character. The double row of street tree plantings and the building massing along the

western edge of the Rotary reinforces the circular form. The design of the rotary shall be formal and symmetrical about the Eisenhower axis. There should be a focal element at the center of the rotary such as a statue.

The plantings should respond to the formal nature of the space, however, plants should be selected to relate to the streetscape and the existing vegetation in the park. A strong relationship between the Rotary and the park is encouraged.

Treatment of the streetscape on the inside edge of the rotary shall be as designed with the rotary.

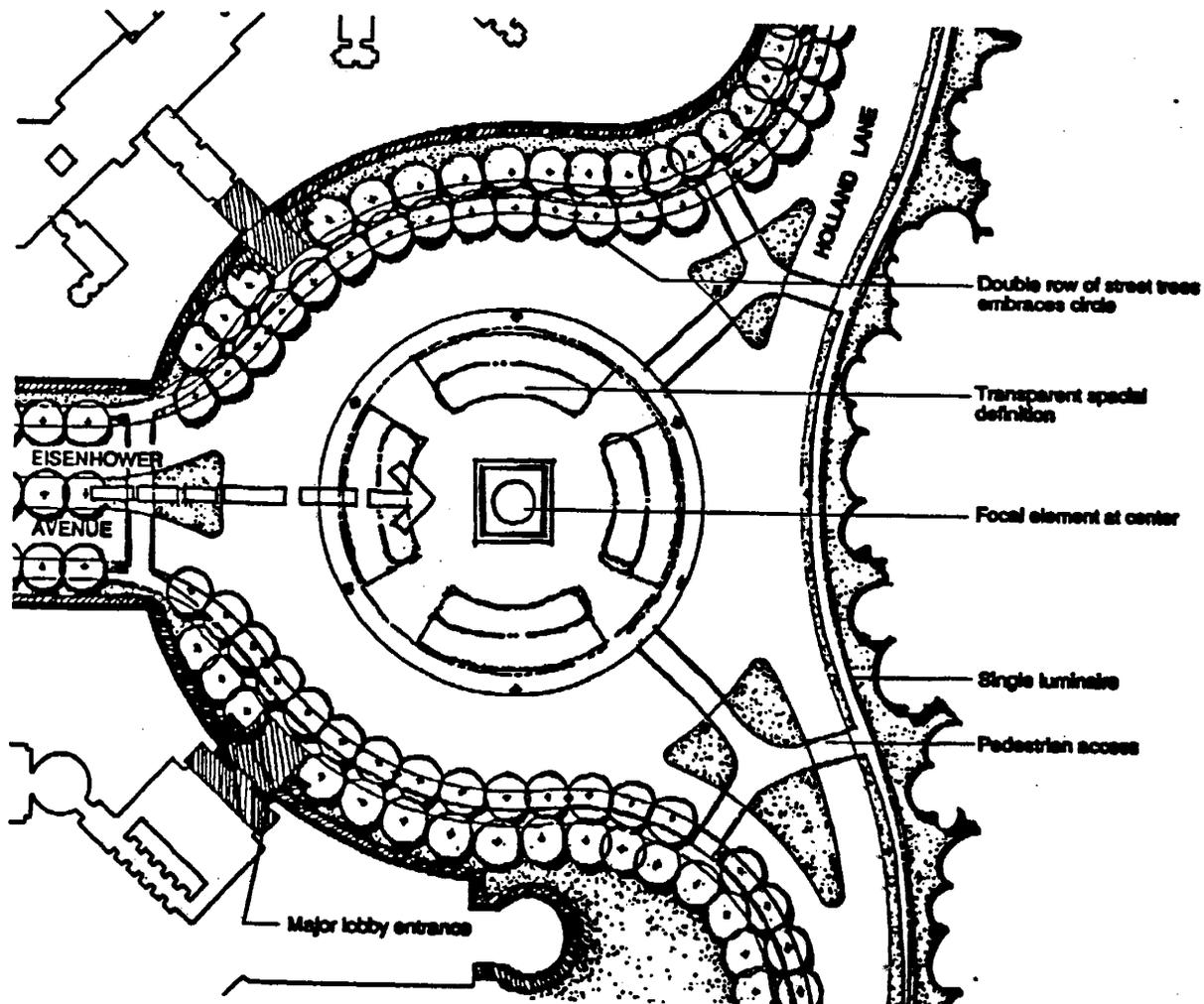


Figure 7-4: Rotary Open Space Concept



7.2.2 ALEXANDRIA AFRICAN AMERICAN HERITAGE PARK

The nine acre Alexandria African American Heritage Park is the largest open space in Caryle. It will "commemorate individuals, groups, events, achievements and contributions by Black Americans who have had a marked effect on the City's character, development and history" (Cooper Robertson and Partners).

The design for the park should be clearly related to the streetscape along the Holland Lane edge. Because of the existing trees, no additional street trees shall be planted along the east side of Holland Lane. This will reinforce the visual connection to the park from Holland Lane and enhance the pastoral setting. Continuous sidewalk and street lighting however must be provided along Holland Lane; Refer to Section 7.3 Streetscape Design for the specific requirements. The pedestrian crosswalks of Holland Lane at the three intersections are strong pedestrian links to the park. These should also be reinforced in the park design.

7.3 STREETScape DESIGN

7.3.1 TYPICAL LAYOUT AND DIMENSIONS

In Holland Precinct, the streetscape configuration is designed to enhance the parkway image. Because of the park on the east side of Holland Lane the streetscape configuration is not symmetrical. On the west side of Holland Lane, adjoining the residential buildings, the usual three streetscape zones will be strictly defined.

The curb zone will be a seven foot wide area within the right-of-way with street trees centered. This zone shall be planted with lawn with the street trees

centered in the lawn. The pedestrian zone shall be defined as an eight foot wide sidewalk located immediately adjacent to the curb zone; the sidewalk shall have an eight inch band on each side. The building zone shall be the area from the edge of the sidewalk to the building facade, approximately fifteen feet. This area shall be landscaped with trees, shrubs, groundcover and lawn as required in Section 7.8 - Planting, and may be occupied by a portion of a garden court entrance.

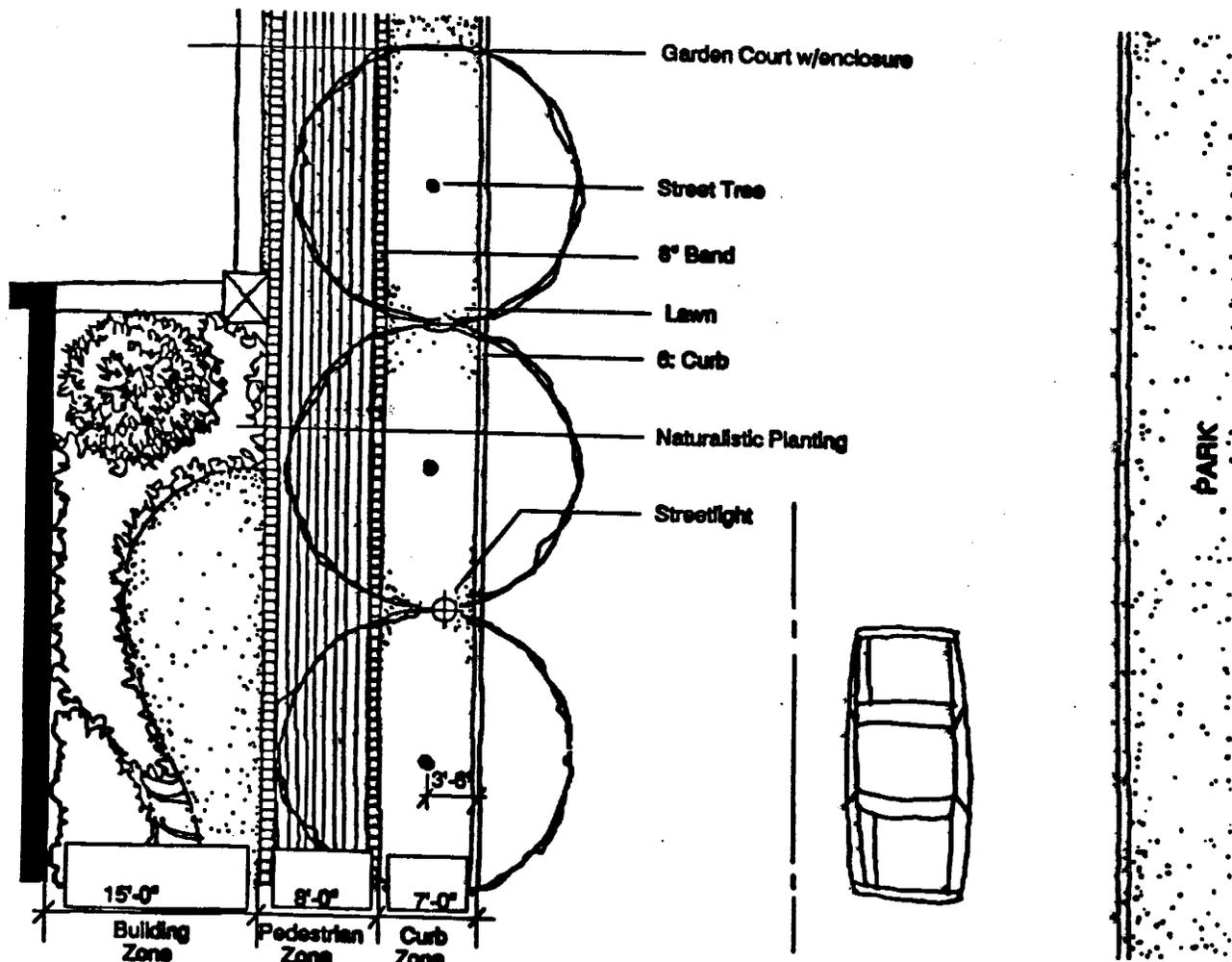


Figure 7-5: Holland Precinct Typical Street Layout



Figure 7-6: Garden Court

7.3.2 BUILDING ENTRANCES

MAJOR LOBBY ENTRANCE: Major lobby entrances of buildings facing the Rotary may be designed by the parcel owner, however, they may not interrupt the curb zone. No physical connection to the street with pavement will be permitted and the street trees within the curb zone shall be maintained at the specified spacing.

GARDEN COURTS: One garden court shall be provided for Blocks H, L and O facing onto Holland Lane. This garden court may extend into the building zone of the streetscape. If the garden court is elevated, a transition to the street shall be provided. The transition shall incorporate stairs with intermediate landings and planters in a manner which is graceful and embraces the street. The transition to the street shall be appropriately scaled to the building architecture and the sidewalk.

7.4 HARDSCAPE

7.4.1 SIDEWALK PAVING

STANDARD SIDEWALK PAVING: The standard sidewalk paving for Holland Precinct shall be the King William Range brick pavers in a running bond pattern parallel to the street. An eight inch rowlock band shall be provided along each edge of the sidewalk.

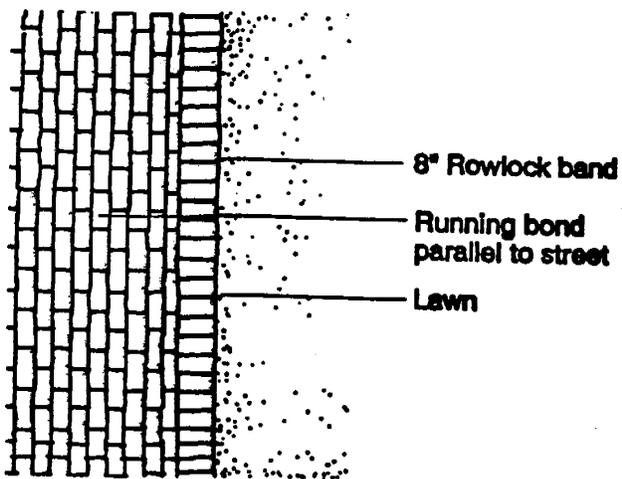


Figure 7-7: Holland Precinct Standard Sidewalk Paving



7.5 STREET FURNITURE

In order to enhance the pastoral nature of Holland Precinct, street furniture shall be kept to minimum. The following requirements apply in addition to those in Chapter Two.

7.5.1 SEATING

FORMAL SEATING - BENCHES: Benches shall be provided in garden courts in a quantity appropriate to the design. No other benches are permitted within the streetscape.

7.5.2 STREET FIXTURES

TRASH RECEPTACLES: One trash receptacle shall be provided in the curb zone at all street corners only.

FENCES, GATES, COLUMNS AND WALLS: The use of fences, gates, columns and walls as a means to provide the required enclosure for a Garden Court is encouraged. Fences and columns shall allow visual access to the garden court from the sidewalk.

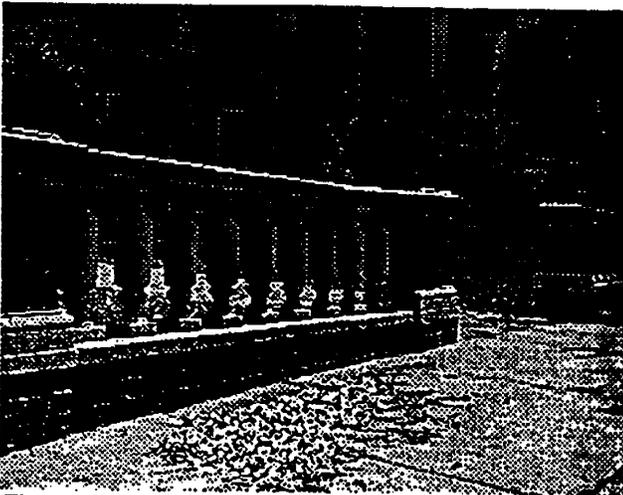


Figure 7-8: Balustrade Fence

7.6 LIGHTING

7.6.1 STREETLIGHTS

All streetlights in Holland Precinct shall be a single luminaire streetlight as specified in the General Streetscape Guidelines. Streetlights along Holland Lane shall be spaced at eighty feet on center and staggered across Holland Lane. Streetlights along Eisenhower Avenue shall be spaced at eighty feet on center and paired across Eisenhower Avenue. Streetlights around the Rotary shall be spaced at eighty feet on center and arranged in a formal manner.

At the intersections of Emerson and Ballenger Streets with Holland Lane, streetlights shall be placed to maximize vehicular and pedestrian safety. For example, a single luminaire may be placed on axis with the intersecting street, or streetlights may be placed on all four corners of the intersection.

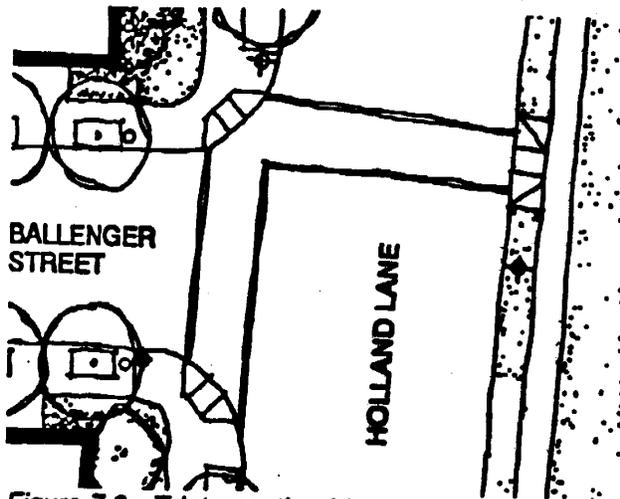


Figure 7-9: T Intersection Lighting



7.7 PLANTING

7.7.1 STREET TREES

Holland Precinct shall have two street trees. They shall be spaced at twenty feet on center.

Along the west side of Holland Lane, a single row of Red Oaks, *Quercus rubra*, shall be provided. Beginning at the point of curvature on Holland Lane before it joins the Rotary and around the western sides of the rotary, a double row of Bloodgood Plane Trees, *Plantanus x acerifolia*, shall be provided. The second row shall be fifteen feet behind the first row and shall be paired radially with the first row along the arc. All street trees in the curb zone in Holland Precinct shall be planted in lawn. There will be no street trees along the east side of Holland Lane.

In addition, a single row of Bloodgood Plane Trees shall be planted along the centerline of the median of Eisenhower Avenue between Blocks O and P.

7.7.2 PLANTING BEDS AND PLANTERS

Continuous at-grade planting beds shall be provided along 100% of the net facade length in the building zone against all buildings.

ROTARY AND EISENHOWER AVENUE FACADES: Planting beds along the Rotary and Eisenhower facades shall reinforce the formal geometric form of the rotary. Planting beds shall be a minimum of five feet wide and may extend to the full width of the building zone; however, a minimum of twenty-five percent of the building zone area shall be lawn.

Plant selections shall be native indigenous plants that relate to the existing vegetation in the park.

HOLLAND LANE FACADES: Planting beds along Holland Lane shall be designed in a naturalistic, pastoral manner to reinforce the visual connection across the street to the park. The planting beds shall be a minimum of five feet wide and may extend to the full width of the building zone; however, a minimum of thirty percent of the building zone shall be lawn. Mainly lower scale understory plantings should be used. The use of smaller shade trees,

ornamental and flowering trees, and large shrubs is encouraged. The planting shall include a mix of evergreen and deciduous plants; plants shall be native or indigenous plants to relate to the existing vegetation in the park.

7.7.3 FREESTANDING PLANTERS

Freestanding planters are not permitted except in association with garden courts.

7.8 SIGNS

7.8.2 INFORMATION SIGNS

BUILDING IDENTIFICATION SIGNS: Since the primary entrances to the residential buildings are located within Ballenger Precinct, the primary signs shall be located there. Smaller secondary building identification signs may be provided, however, in association with garden courts only.



7.11 TRANSITIONAL AREAS

7.11.2 TRANSITIONS BETWEEN PRECINCTS

JAMIESON PRECINCT TRANSITION: Holland Precinct shall be the dominant precinct in the transition to Jamieson Precinct. The street trees shall remain the same as specified along each street and extend to the corners. The sidewalk pavement will change from the Holland standard paving to the Jamieson standard paving at the extension of the Holland Lane build to line. Plantings for Holland Precinct shall change to Jamieson Precinct. See Figure 7-10.

BALLENGER PRECINCT TRANSITION: Holland Precinct shall be the dominant precinct in the transition to Ballenger Precinct.

At Ballenger and Emerson Streets, the Holland Precinct sidewalk paving shall change to the Ballenger precinct paving at the point of curvature of the curb on the east west streets. See Figure 7-11. The street trees shall be the same on their respective streets. Plantings for Holland Precinct shall extend around the corner for a minimum of ten feet ending at an appropriate architectural feature.

At John Carlyle Street and Eisenhower Avenue, Holland Precinct shall be the dominant precinct. The first two street trees along John Carlyle Street shall be Bloodgood Plane Trees and the first tree shall align with the second row of trees from the curb along Eisenhower Avenue. All street trees beyond that shall be the Ballenger Precinct street tree. The standard sidewalk paving for Holland Precinct shall extend to the line of the sidewalk in front of Block N extended across John Carlyle Street. Plantings in the building zone shall extend a minimum of ten feet around the corner from Eisenhower Avenue and end at an appropriate architectural feature. See Figure 7-12.

EISENHOWER PRECINCT TRANSITION: Holland Precinct and Eisenhower Precinct streetscape design is similar. The transition between the two precincts at John Carlyle Street shall be made so that neither is dominant, and the experience of Eisenhower Avenue is continuous. See Figure 7-13. At Block P, the first two street trees along John Carlyle Street shall be Bloodgood Plane trees to match those along Holland; the first tree shall align to the second row of street trees along Eisenhower Avenue.

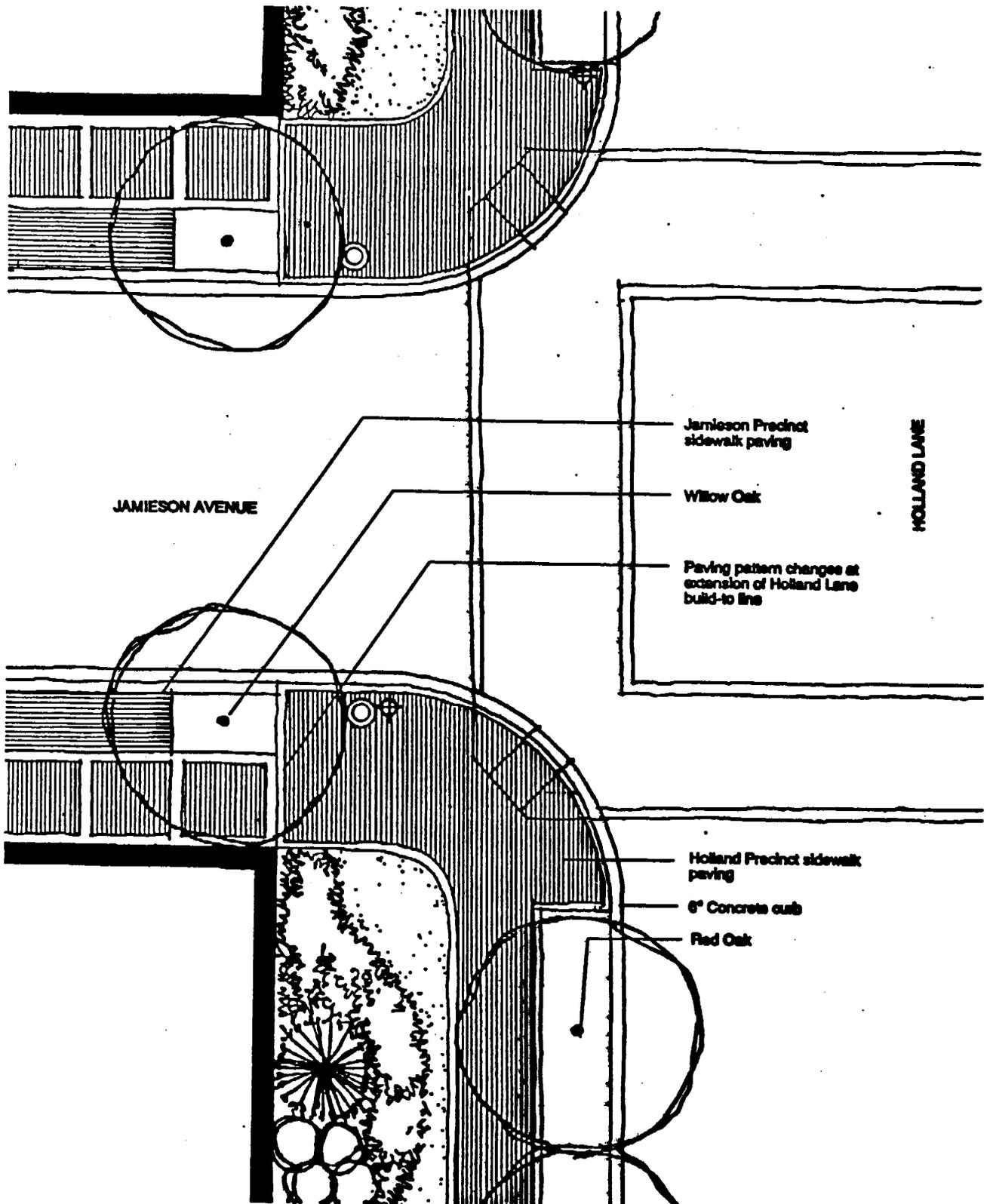


Figure 7-10: Jamieson Precinct Transition

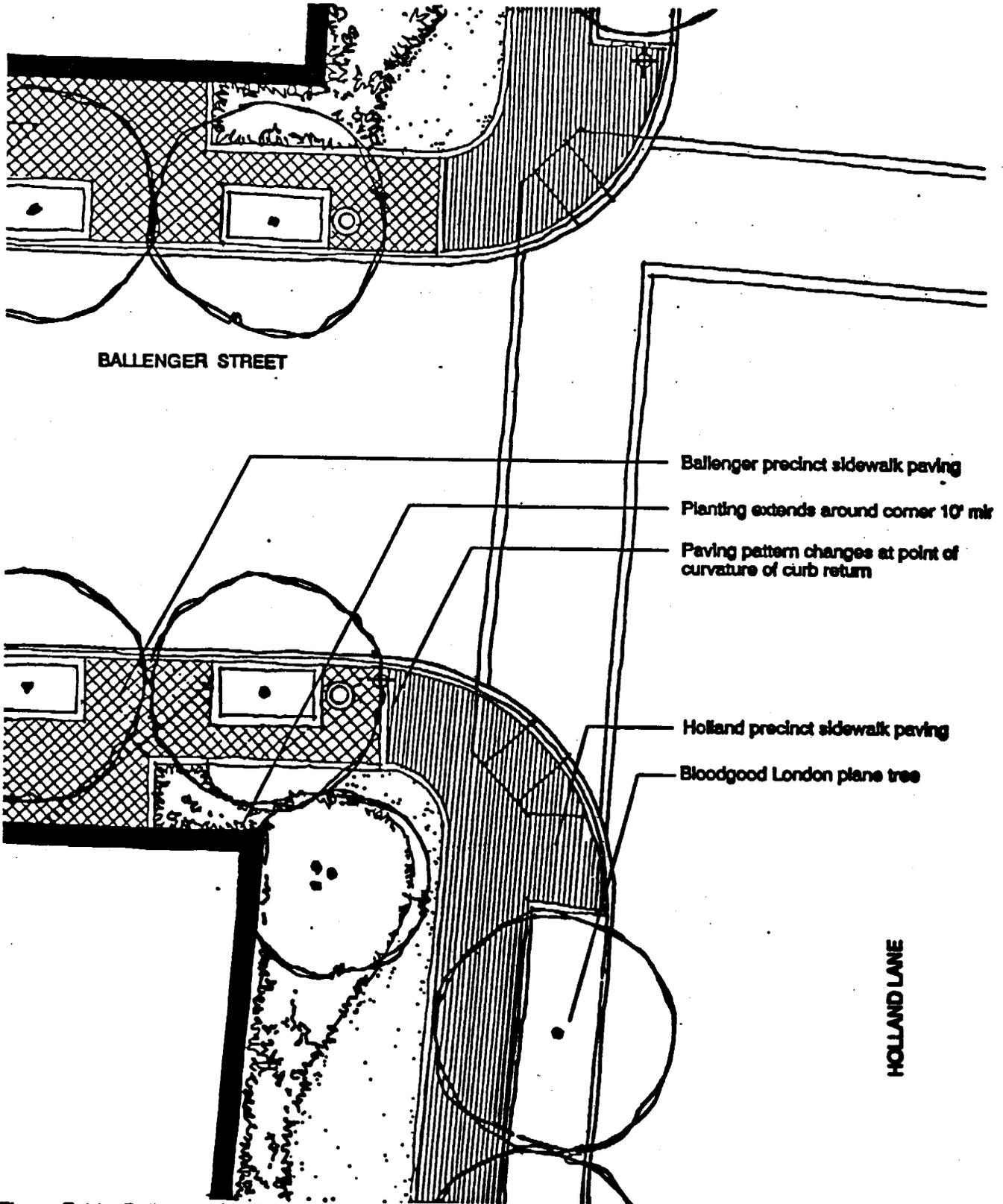


Figure 7-11: Ballenger Precinct Transition at Ballenger and Emerson Streets

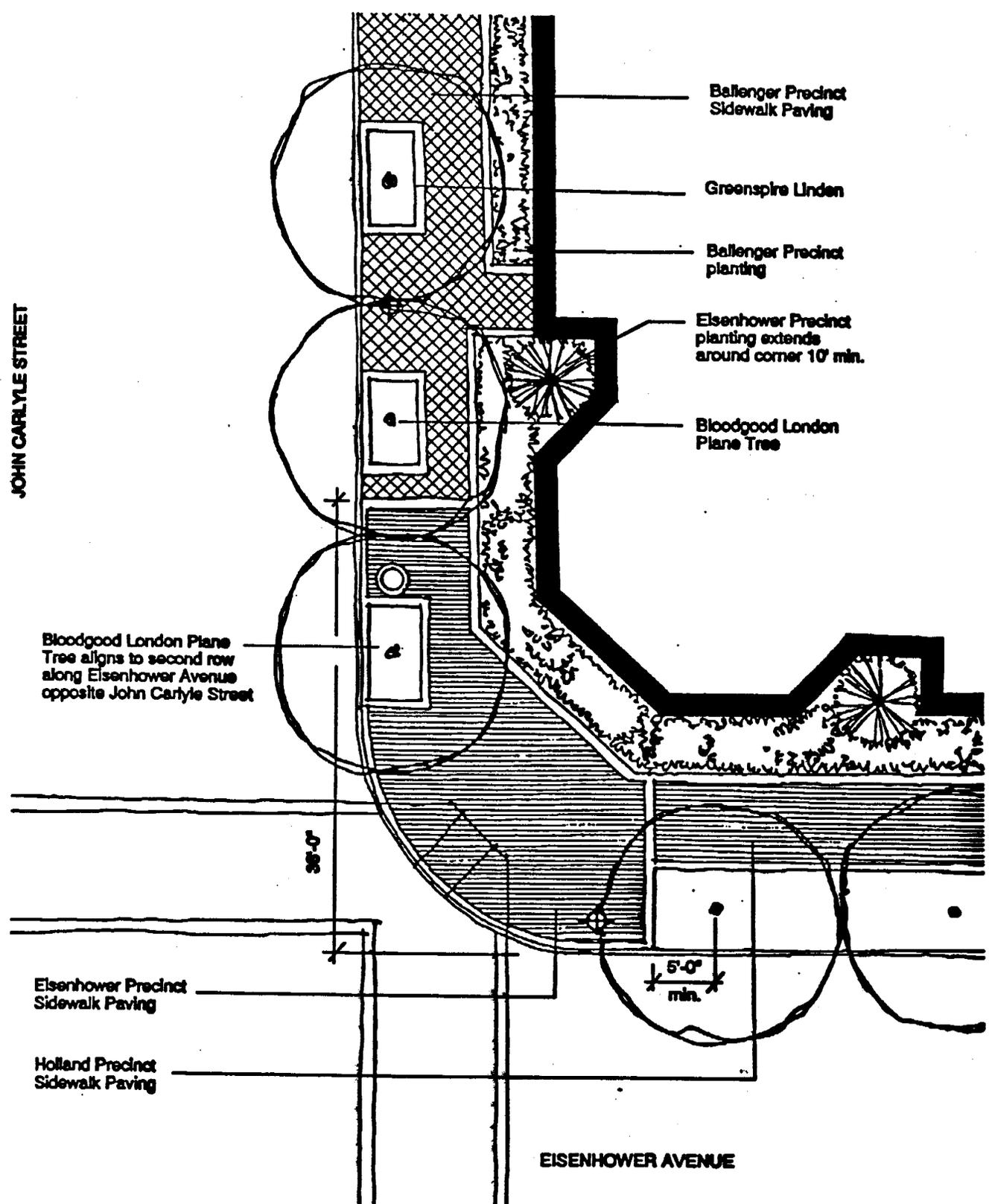


Figure 7-12: Ballenger Precinct Transition at Eisenhower Avenue

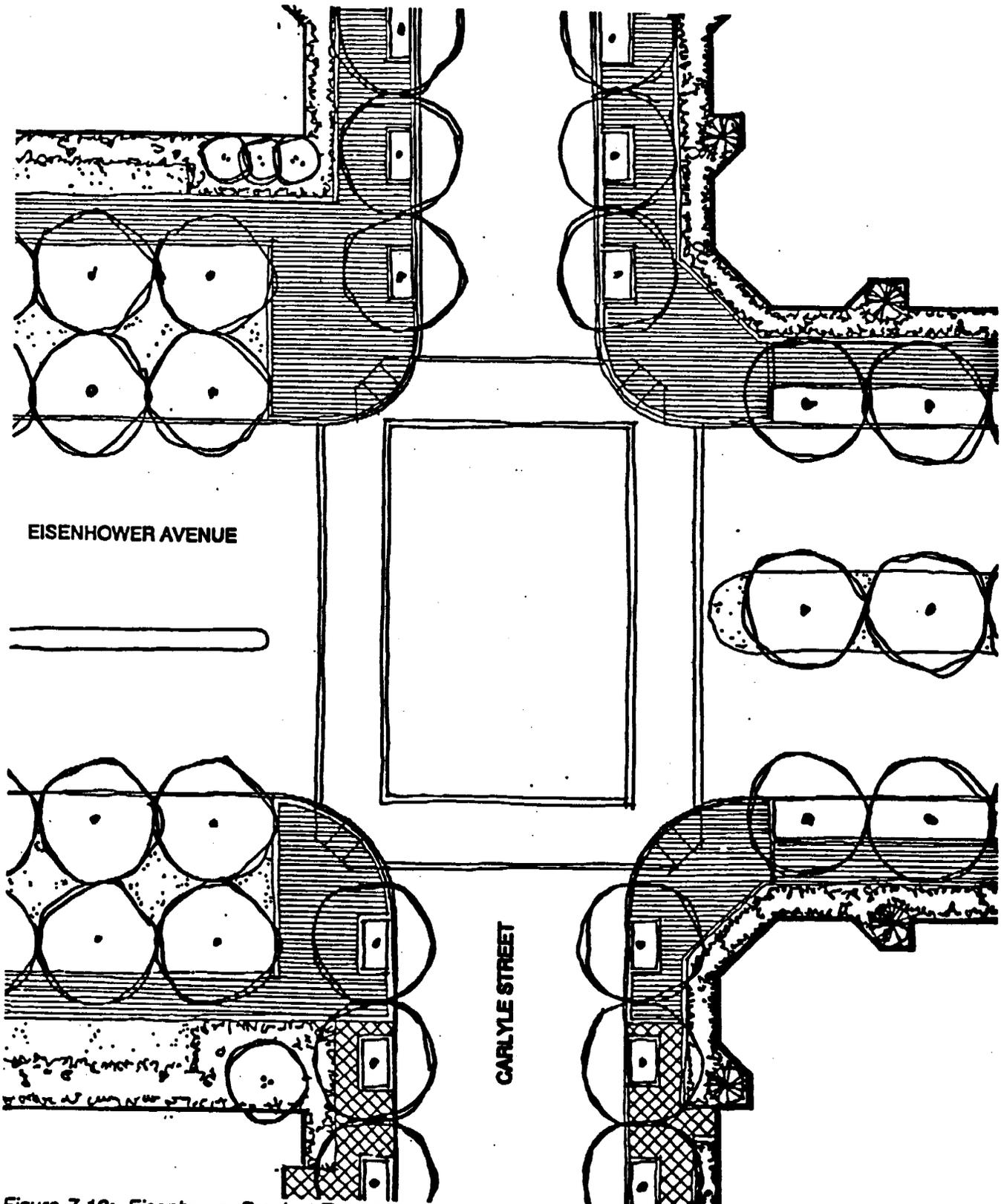


Figure 7-13: Eisenhower Precinct Transition

8 BALLENGER PRECINCT

Additional information and special requirements for the streetscape for Ballenger Precinct are outlined below. These guidelines shall apply in addition to the General Streetscape Guidelines; when a topic is not addressed, the General Streetscape Guidelines shall apply.

8.1 Design Concept and Character	8-3	8.8 Signs	8-12
8.1.1 Context	8-3	8.8.2 Information Signs	8-12
8.1.2 Use	8-3	•Building Identification Signs	
8.1.3 Imagery	8-4	8.11 Transitional Areas	8-13
8.2 Major Open Space	8-5	8.11.2 Transitions between Precincts	8-13
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8.3.1 Typical Layout and Dimensions	8-7	•Eisenhower Precinct Transition	
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•Pavement Bed Edges			
8.5 Furniture	8-9		
8.5.1 Seating	8-9		
•Formal Seating: Benches			
8.5.2 Street Fixtures	8-9		
•Trash Receptacles			
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8.6 Lighting	8-10		
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8.7.3 Freestanding Planters	8-11		



8 BALLENGER PRECINCT

Ballenger Precinct will be home to most of Carlyle's residents, as it is a primarily residential precinct. A combination of pedestrian oriented open spaces and a variety of buildings will make it an interesting place to stroll. Ballenger Precinct will best exemplify the first design principle of the Development Plan which states that the number and kinds of buildings will be diverse. Individual expressions of homes will add a charm and character to the precinct that will be reminiscent of Old Town.

Ballenger Precinct is located on the east side of Carlyle. It is enclosed by Carlyle Precinct to the north, Holland Precinct to the east, Eisenhower Precinct to the south and Dulany Precinct to the west.

8.1 DESIGN CONCEPT AND CHARACTER

8.1.1 CONTEXT

SITE PLAN: The residential blocks of Ballenger Precinct are visually connected not only by the streets but by the open spaces within the precinct. Ballenger Precinct will have a mix of four story buildings with seven, twelve and fifteen story towers with major lobby entrances. The street wall will be animated with cut outs, balconies and bay windows.

CIRCULATION: Vehicular circulation in Ballenger Precinct will access three blocks in Ballenger Precinct at drop off courts adjacent to the towers allowing a slower zone of traffic to penetrate the residential blocks. Pedestrians will have many routes to choose in the Precinct. The major open space will provide cross block connections.

OPEN SPACE: Ballenger Mews, a cruciform space formed by the massing of the buildings is the focus of Ballenger Precinct. The mews links blocks H, K, L and O on the cross axes. The link crosses Ballenger and Emerson Streets connecting with the dropoff courts on the either side. This link forms an important visual and physical relationship for the

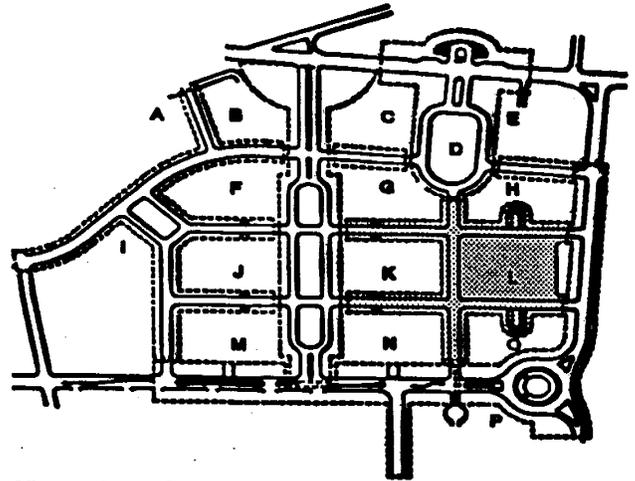


Figure 8-1: Ballenger Precinct

buildings in Blocks H and O to the Mews. The streetscape should reflect this open space extension.

Ballenger Mews is more inwardly focused than any of the other open spaces in Carlyle. Unlike the other open spaces, it is not surrounded entirely by streets. The adjoining residential buildings will give it a more private and intimate feel. A day-care center may also be located adjacent to the Mews.

8.1.2 USE

The predominant use in Ballenger Precinct is residential. The main use of the streetscape will be the movement of people to and from work, to school, shopping and other daily routines. The streetscape could also be a place where children play while their parents sit on the steps of their house and chat with the neighbor. Leisurely strolling, walking the dog and pushing the baby carriage will also be a Ballenger Precinct activity. Most of the use of the streetscape will be fairly constant through the daylight hours with the use tapering off in the evening. The uses in Ballenger Precinct are semi public to private and, as in most residential neighborhoods, strangers are not as welcome.



Figure 8-2: Williamsburg, Virginia

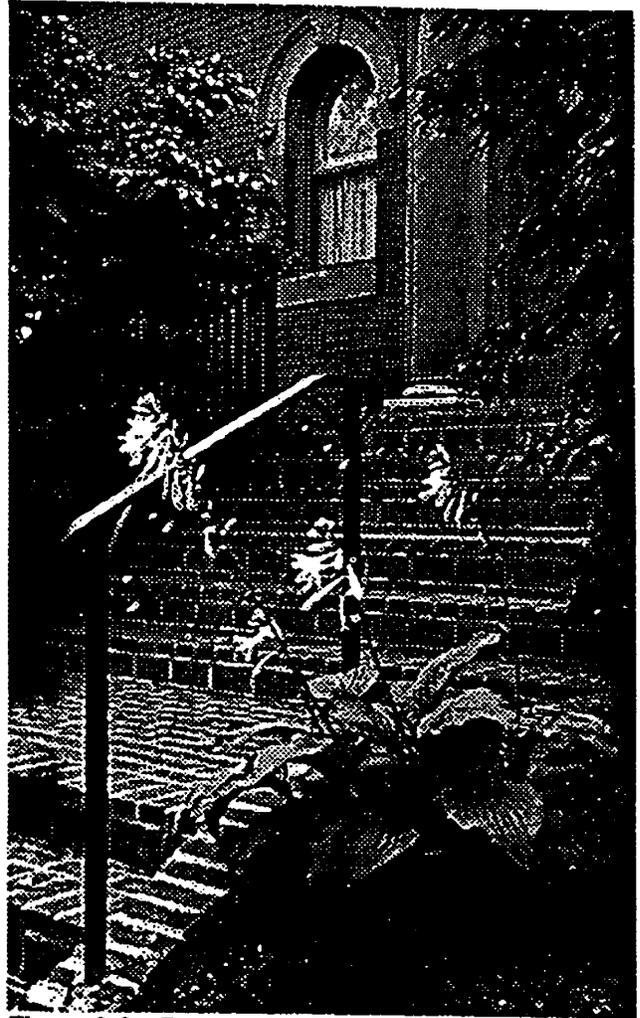


Figure 8-3: Residential Plantings

8.1.3 IMAGERY

Ballenger Precinct should invoke a comfortable, welcoming feeling of home with the associated sense of security. Varied architectural expressions and a varied landscape palette will give Ballenger a personal and residential feel. The street should be full of human scale details such as door knockers, stoops, shutters, window boxes, planters and pots.



8.2 MAJOR OPEN SPACE

The centerpiece of Ballenger Precinct is Ballenger Mews, a one and a half acre park. This open space will be a richly planted garden with contoured lawns and curvilinear walks. The Mews will be similar in character to Louisburg Square in Beacon Hill in Boston or the squares in Charleston, South Carolina. The Mews should be designed to be intimate in scale and naturalistic.

The cruciform shape of the Mews creates axial relationships to the buildings at each end. The cross axis extends from Block L into the residential courts in Blocks H and O. The street trees along Ballenger and Emerson Streets shall break to reinforce the relationship of the Mews to the courts. In addition, the residential courts in Blocks H and O shall be designed to focus onto the mews as well. See 8-4.

Vehicular and pedestrian entrances in the Mews and the residential courts shall be emphasized equally. Vehicular access areas shall be treated as pedestrian plazas. The areas adjoining the streets shall be fenced with transparent fencing to enhance the semiprivate feel of the mews while maintaining visual access from the street. The plantings in the Mews shall be layered in a definite hierarchy. Plantings shall include shade trees, ornamental trees, shrubs, flower beds and specimen plants. The greater portion of the ground plane shall be lawn.

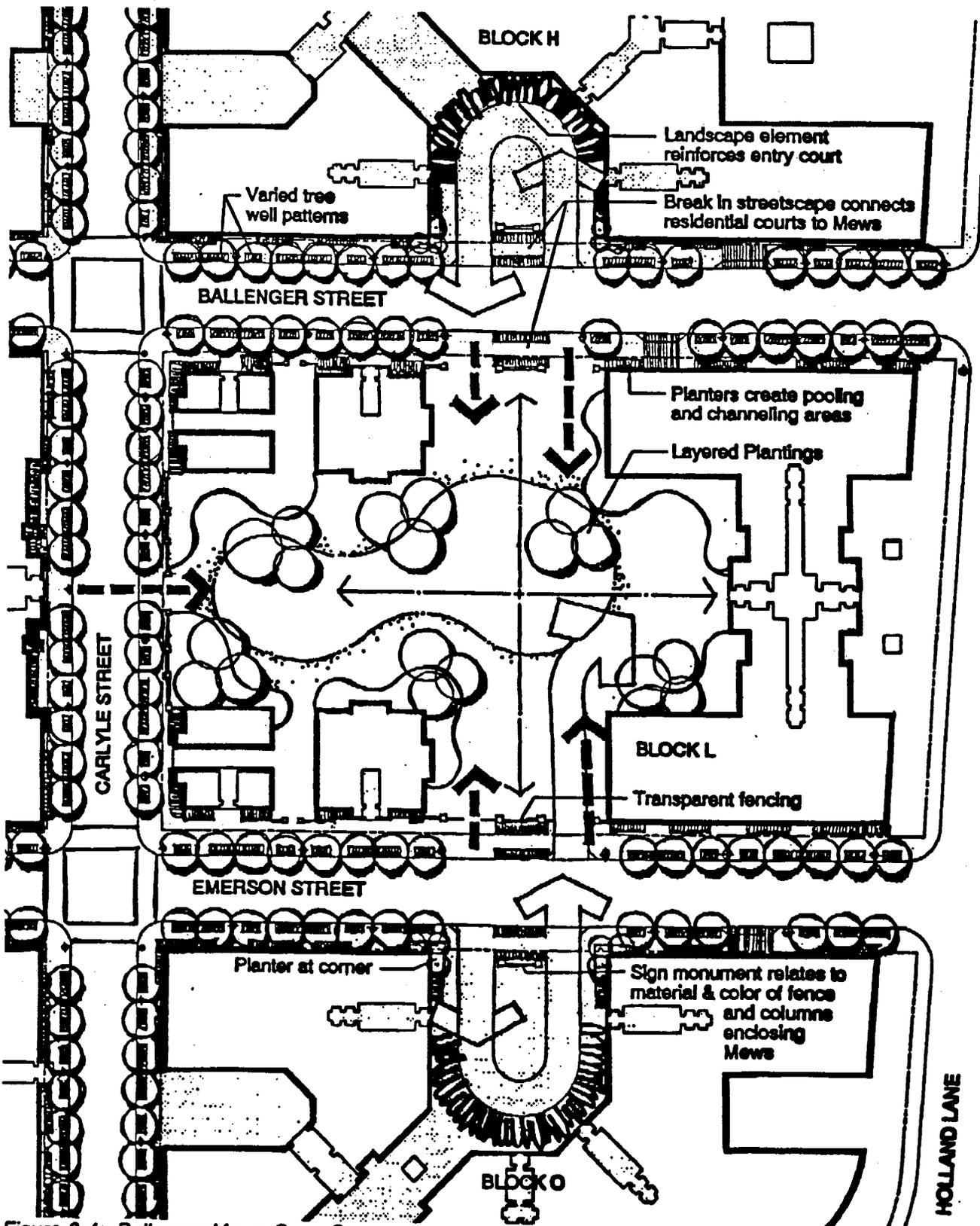


Figure 8-4: Ballenger Mews Open Space

8.3 STREETScape CONFIGURATION

8.3.1 TYPICAL LAYOUT AND DIMENSIONS

The layout of the streetscape in Ballenger Precinct is designed to encourage variety especially at the interface of the building and pedestrian zones. The experience of walking along the sidewalk should be slightly meandering one, much like in the residential sections of Old Town.

In Ballenger Precinct, the curb zone shall be five and one-half feet wide with a four inch wide band at the back of the curb. The pedestrian zone shall be a minimum of four feet wide and clear of any

obstruction. The building zone width shall vary in combination with the pedestrian zone, allowing the most flexibility to adjust widths of pavement and plantings as needed.

8.3.3 VEHICULAR ACCESS ZONES

DROPOFFS/LAYBYS: Dropoff loops and drives within Ballenger Precinct shall be designed as vehicular accessible pedestrian plazas. Drives shall be flush with walks and have no curbs. The intersection of the drive and the street shall be treated as a parking/service entrance.

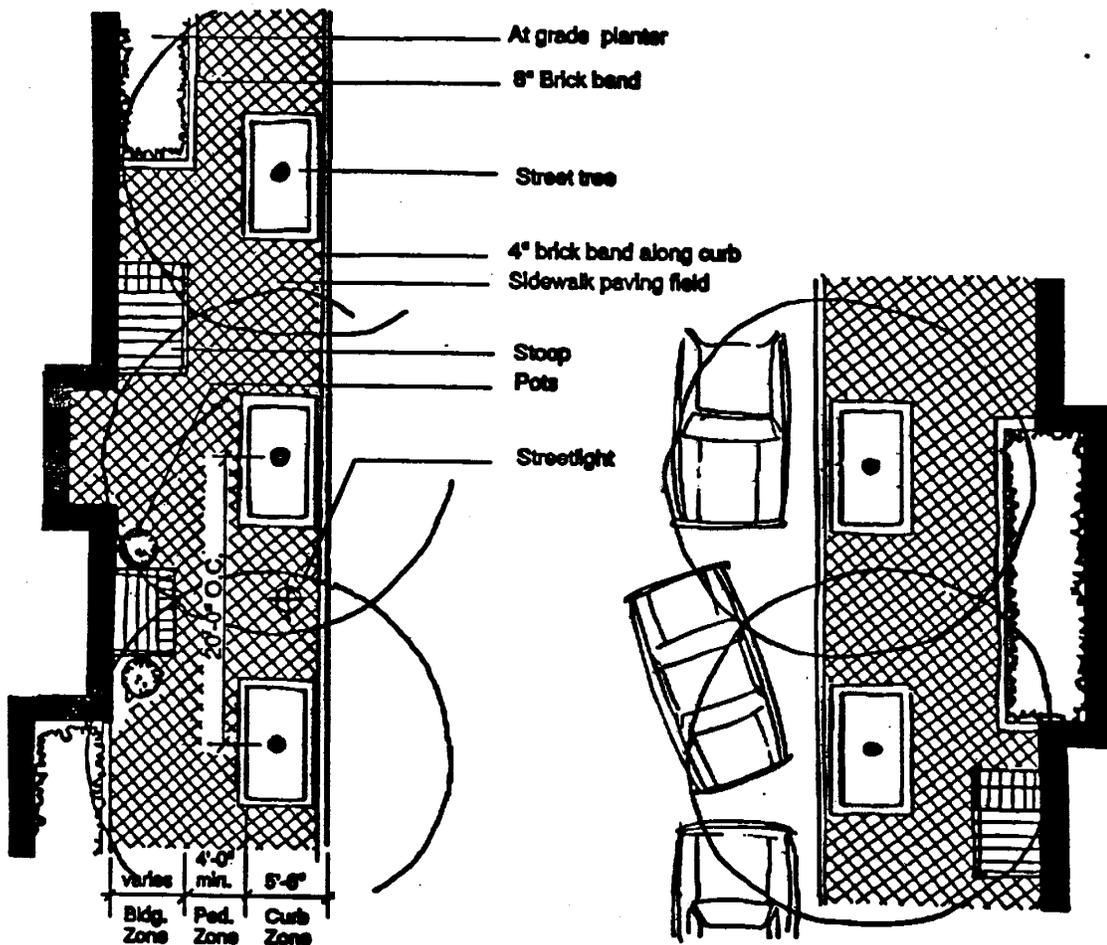


Figure 8-5: Ballenger Precinct Typical Streetscape



8.4 HARDSCAPE

8.4.1 STANDARD SIDEWALK PAVING

STANDARD SIDEWALK PAVING: The standard sidewalk paving for Ballenger Precinct shall be the King William Range brick paver. The brick pavers shall be laid in a herringbone pattern which is aligned at forty-five degrees to the curb. A single stretcher course band shall be installed along the curb and a single row stack bond band shall be used to edge all flush at grade planters including street tree pits and building zone plantings.

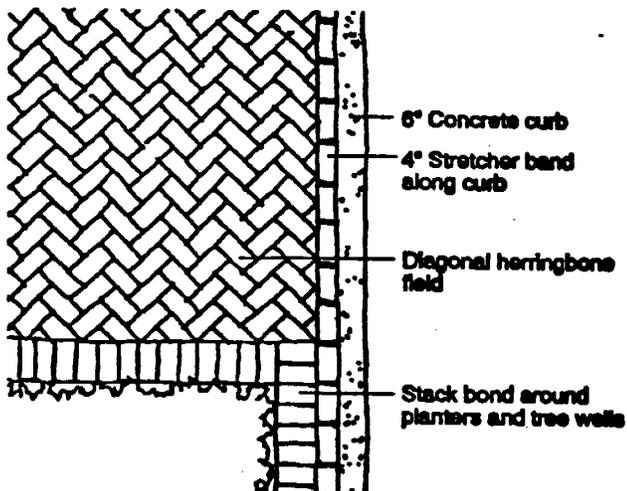


Figure 8-6: Ballenger Precinct Standard Sidewalk Paving

PAVEMENT EDGES: All planter bed and tree well edges shall be flush and at-grade.

8.4.2 CURBS

All curbs in Ballenger Precinct shall be six inch concrete curb and gutter.



8.5 FURNITURE

8.5.1 SEATING

FORMAL SEATING - BENCHES: Benches may be used in open spaces and garden courts only.

8.5.2 STREET FIXTURES

TRASH RECEPTACLES: Two trash receptacles shall be provided at each intersection within Ballenger Precinct. Two trash receptacles shall also be provided within fifty feet of the three street entrances to Ballenger Mews; additional trash receptacles may be provided as necessary.

FENCES, GATES, COLUMNS AND WALLS: The use of fences, gates, columns and walls to define gardens, private residential entrances and semiprivate courtyards is encouraged in Ballenger Precinct.



Figure 8-7: Residential Fence



8.6 LIGHTING

8.6.1 STREETLIGHTS

Single luminaire streetlights as specified in the General Streetscape Guidelines, shall be spaced at eighty feet on center and shall be staggered across the street in Ballenger Precinct. House side shields shall be installed in streetlights as needed to prevent light from directly entering residential windows.

8.6.2 SPECIALTY LANDSCAPE LIGHTING

The use of specialty landscape lighting is encouraged within Ballenger Precinct especially at residential dropoffs and within Ballenger Mews.

MINIATURE LIGHTS: Permanent miniature lights are prohibited in Ballenger Precinct.

8.6.3 BUILDING LIGHTS

FACADE LIGHTING: Facade lighting will not be permitted in Ballenger Precinct.



8.7 PLANTING

8.7.1 STREET TREES

In order to reinforce the variety desired in Ballenger Precinct, each street has a different street tree. The street tree for John Carlyle Street shall be the Greenspire Linden, *Tilia cordata* 'Greenspire'; for Ballenger Street, the Red Sunset Maple, *Acer rubrum* 'Red Sunset'; and for Emerson Street, the Emerald Queen Norway Maple, *Acer platanoides* 'Emerald Queen'. All street trees in Ballenger Precinct shall be spaced at twenty feet on center.

All other street tree wells in Ballenger Precinct shall be flush with the pavement. These tree wells may be planted with groundcover plantings or seasonal flowers at the discretion of the parcel owner. The parcel owner shall be responsible for the maintenance of the plantings in the tree wells.

8.7.2 PLANTING BEDS AND PLANTERS

Planting beds are required in the building zone for a minimum of eighty percent of the net plantable facade. Planting beds shall be a minimum of three feet wide, and may extend to the full width of the building zone.

The plants below are suggested for use in Ballenger Precinct plantings.

SEASONAL COLOR: Tulips, Hyacinths, Mums, Daffodils, Geraniums, Hosta, Marigolds, Petunias, Ferns and Pansies.

SHRUBS: Azalea, Rhododendron, Wisteria, False Cypress, Witch Hazel

TREES: Dogwood, Magnolias, Cherry, Stewartia, Crape Myrtle and Japanese Silverbell; Spruce, Deodar Cedar, Southern Magnolia and American Holly; Ginkgo, Chinese Elm, English Oak and Zelkova.

8.7.3 FREESTANDING PLANTERS

The use of freestanding planters by individual residents is encouraged.



8.8 SIGNS

8.8.2 INFORMATION SIGNS

BUILDING IDENTIFICATION SIGNS: Freestanding building identification signs may be appropriate to the vehicular dropoffs in Blocks H and O. As stated in the General Streetscape Guidelines, freestanding signs require special approval from Carlyle Development Corporation.



8.11 TRANSITIONAL AREAS

8.11.2 TRANSITIONS BETWEEN PRECINCTS

HOLLAND PRECINCT TRANSITION: Holland Precinct shall be the dominant precinct in the transition to Ballenger Precinct.

At Ballenger and Emerson Streets, the street trees shall remain the same as specified along each street and extend to the corners. The sidewalk layout and pavement shall change from the Holland standard sidewalk paving to the Ballenger standard sidewalk paving at the point of tangency of the curb-return. Plantings and building zone treatments for Holland Precinct shall turn the corner into Ballenger Precinct for a minimum of ten feet changing to Ballenger Precinct standard at an appropriate architectural feature. See 8-8.

At John Carlyle Street and Eisenhower Avenue, the first two street trees along John Carlyle Street shall be Bloodgood Plane Trees and the first tree shall align with the second row of trees from the curb along Eisenhower Avenue. All street trees beyond that shall be the Ballenger Precinct street tree. The standard sidewalk paving for Holland Precinct shall extend to the rear line of the sidewalk at Block N extended across John Carlyle Street to Block O. Plantings in the building zone shall turn the corner extending a minimum of ten feet and ending at an appropriate architectural feature.

EISENHOWER PRECINCT TRANSITION: Eisenhower Precinct is dominant over Ballenger Precinct. the first two street trees on John Carlyle Street shall be Bloodgood London Plane trees to match the Eisenhower Avenue Street Trees. The first tree on John Carlyle Street shall align with the second row of street trees along Eisenhower Avenue. See 8-10. The standard sidewalk layout and paving shall change from Eisenhower Precinct Standard paving to Ballenger Precinct standard paving at the line created by the extension of the rear line of the sidewalk parallel to Eisenhower Avenue. The planting requirements for the building zone for Eisenhower Precinct shall wrap around the

corner extending ten feet minimum along John Carlyle Street and ending at an appropriate architectural feature.

DULANY PRECINCT TRANSITION: Ballenger Precinct is subordinate to Dulany Precinct. The street trees shall be continuous along the respective streets. Standard sidewalk paving will change from Ballenger to Dulany standard sidewalk paving at the extension of the build-to line parallel to Dulany Street. The building zone plantings shall extend to the end of the side building wall for the building that faces onto Dulany Street. See 8-11.

CARLYLE PRECINCT TRANSITION: Carlyle Precinct is the dominant precinct. The standard sidewalk layout and paving shall change from Carlyle Precinct standards to Ballenger Precinct standards at the line created by the extension of the build-to-line across John Carlyle Street. The street trees shall be as specified for the respective streets. The planting requirements for the building zone shall change at the same point as the sidewalk layout and paving. See 8-12.

8.11.3 TRANSITION TO AREAS ADJACENT TO CARLYLE

Ballenger Precinct is entirely within Carlyle; no transitions to areas adjacent to Carlyle is necessary.

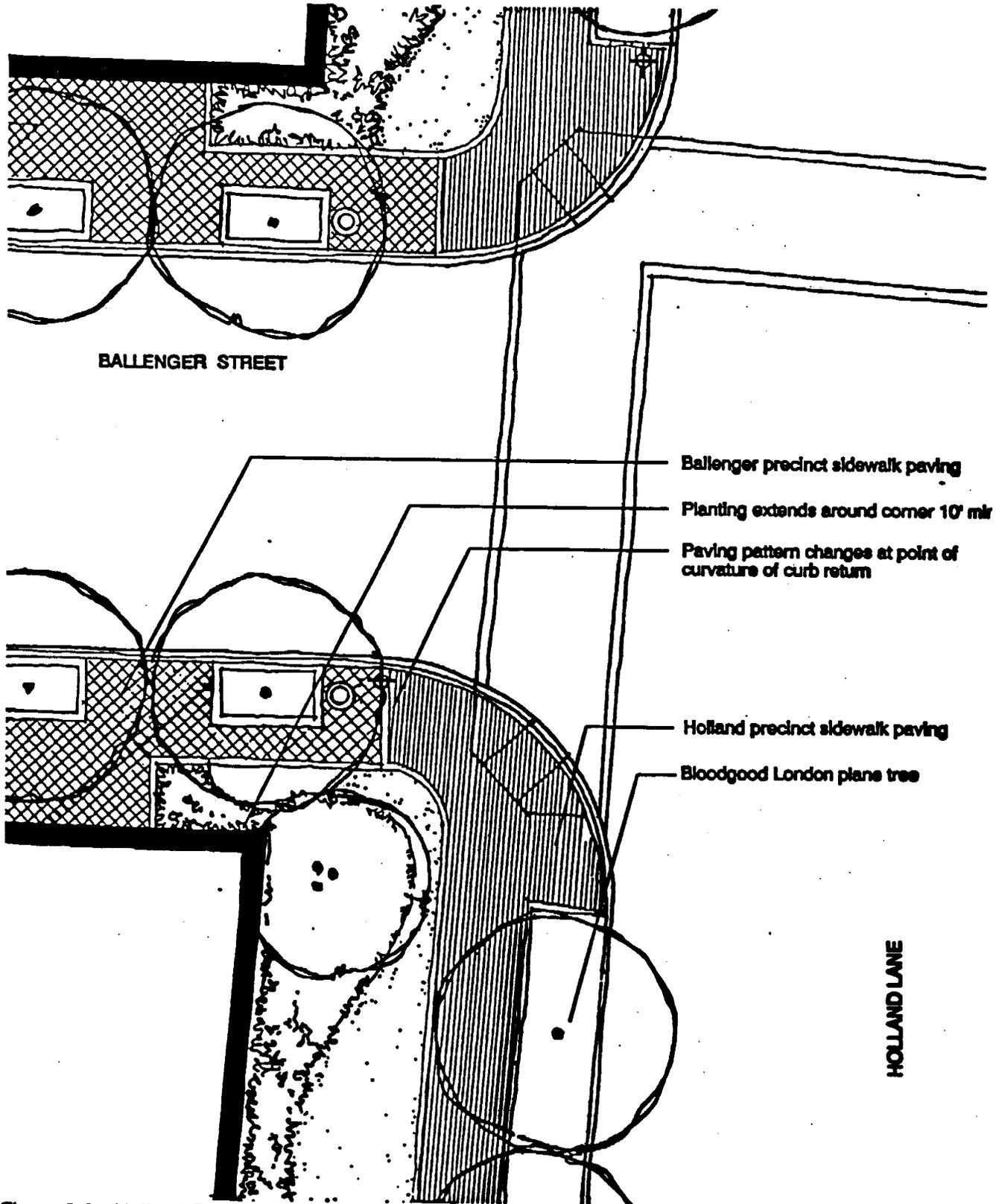


Figure 8-8: Holland Precinct Transition

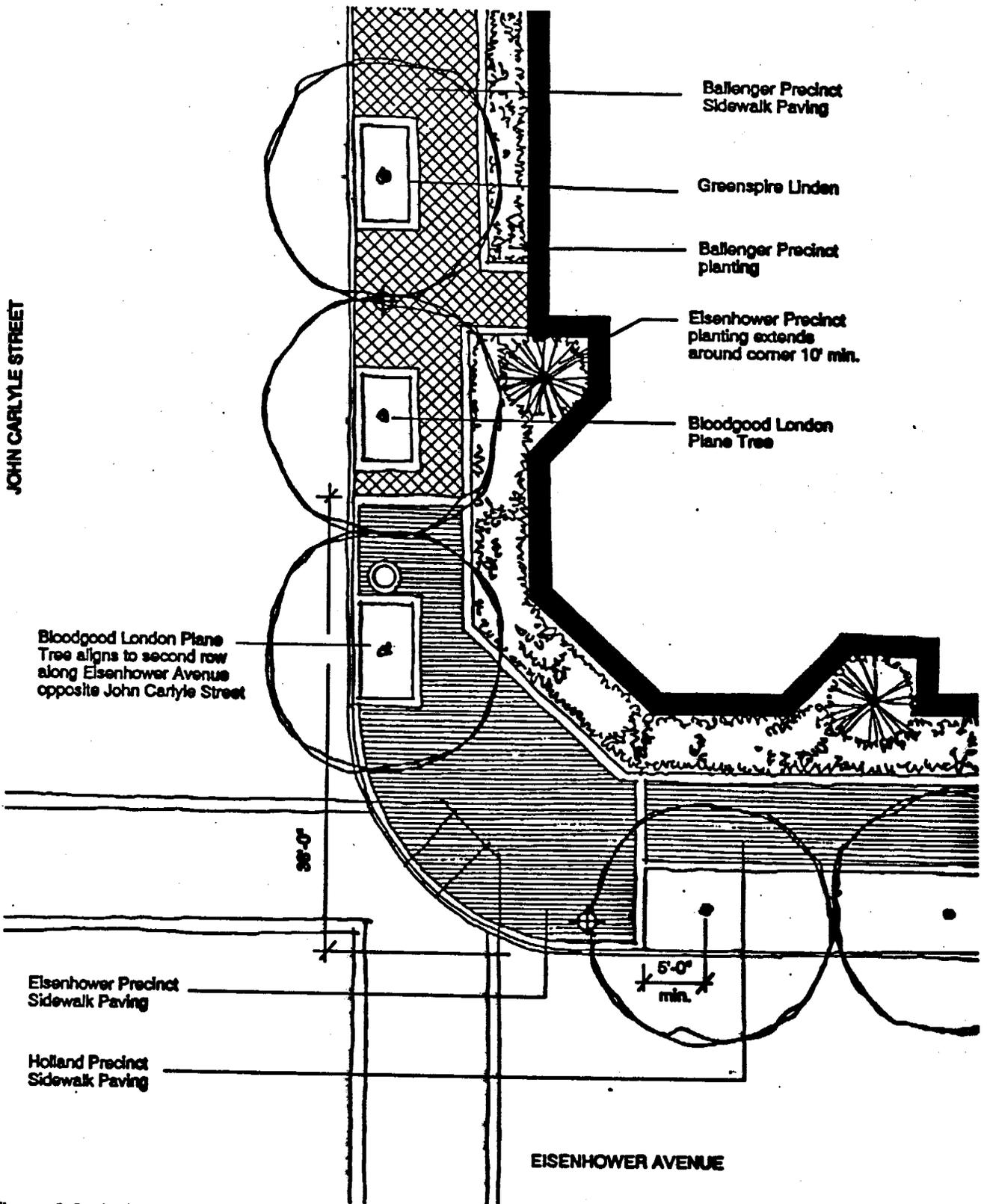


Figure 8-9: Holland Precinct Transition

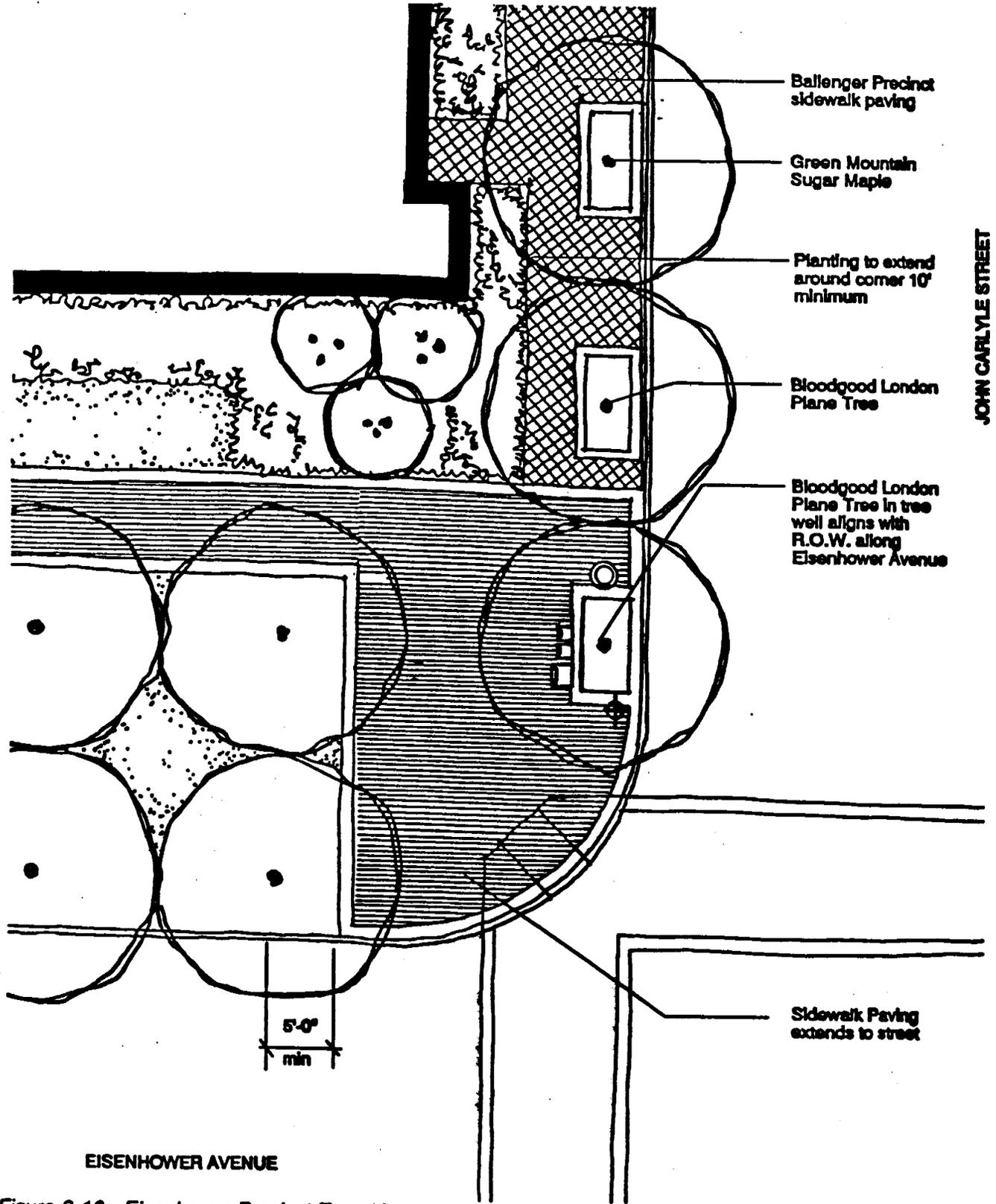


Figure 8-10: Eisenhower Precinct Transition

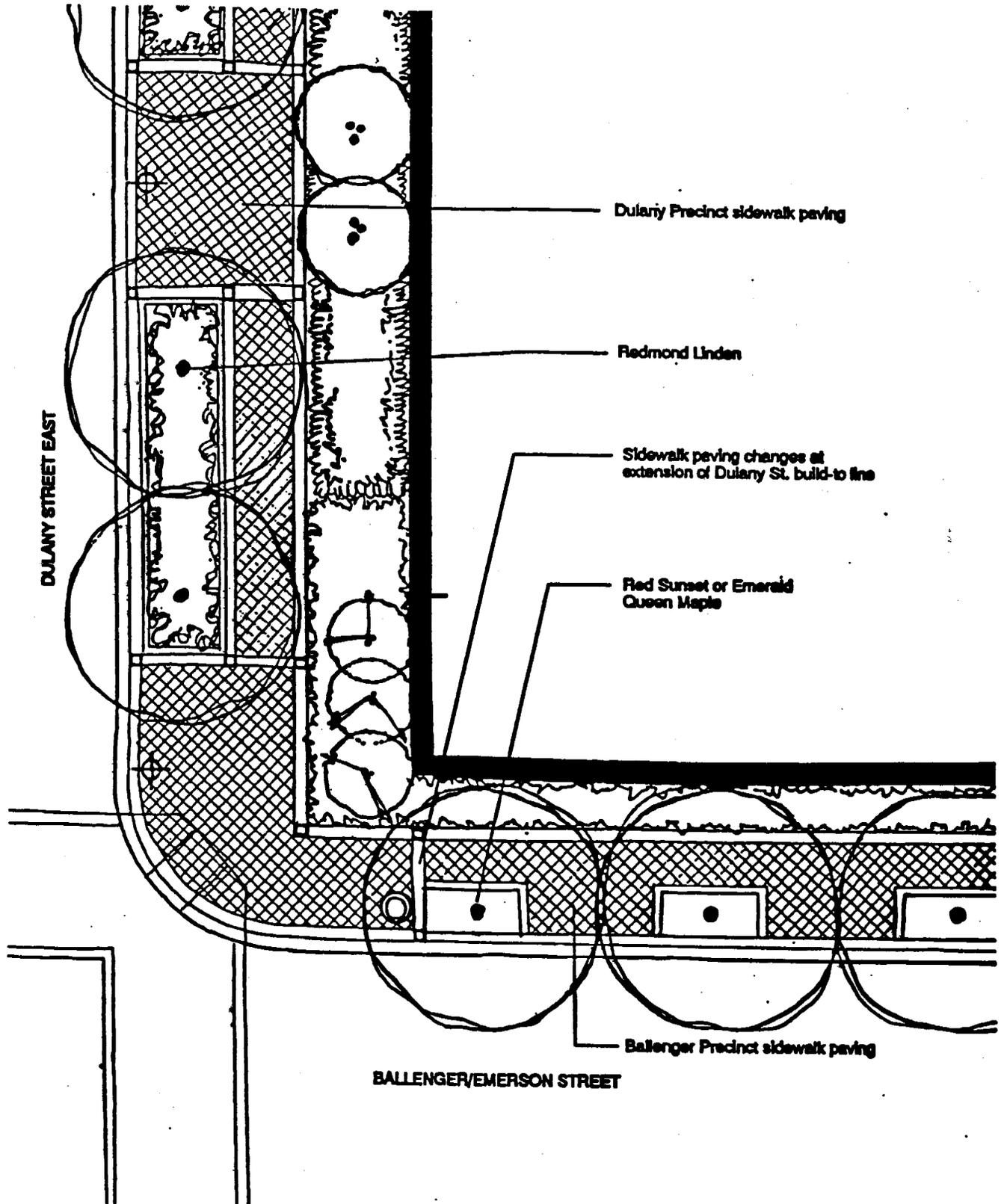


Figure 8-11: Dulany Precinct Transition

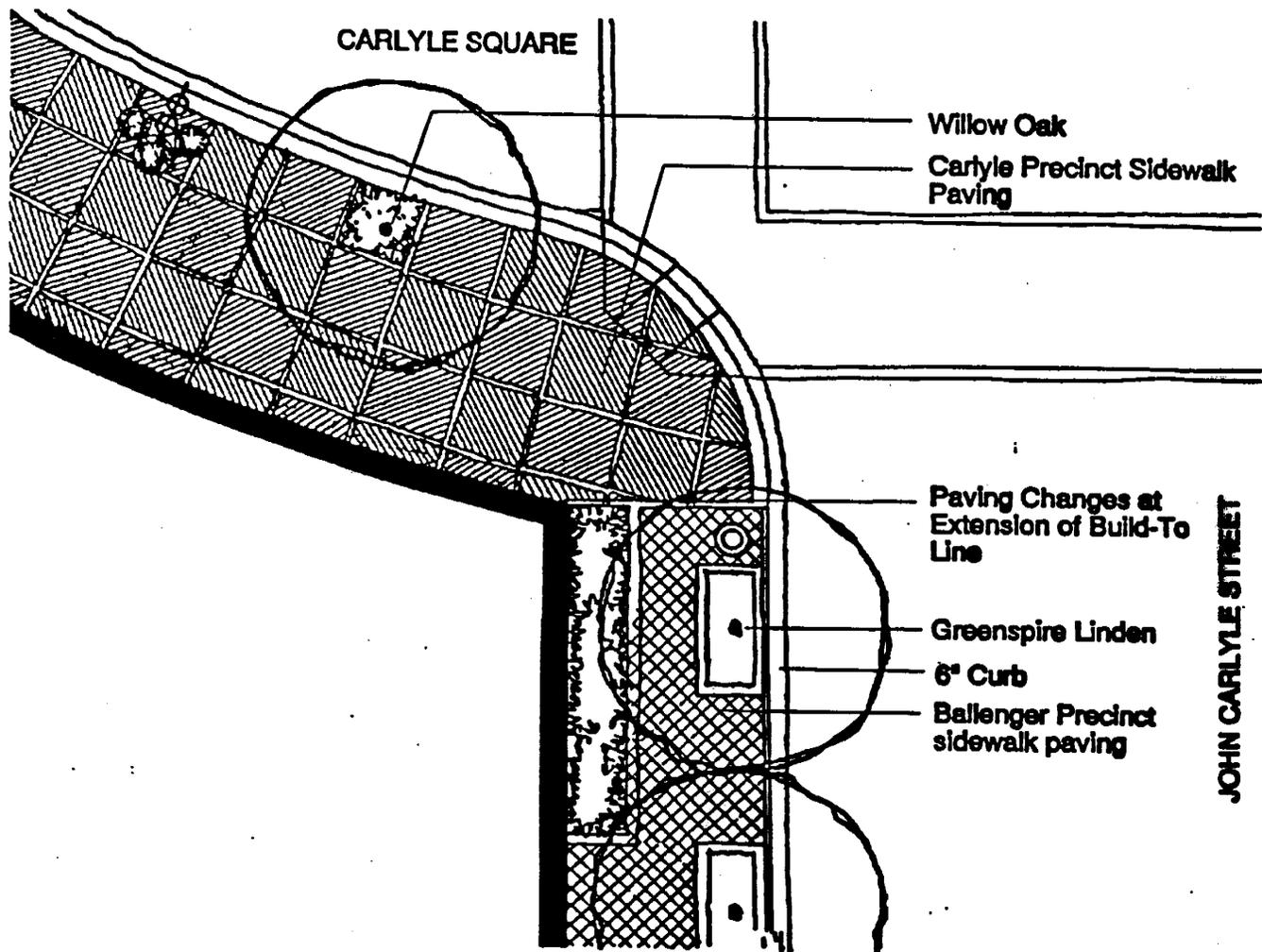


Figure 8-12: Carlyle Precinct Transition

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